



**Illinois Department
of Transportation**

**Local Project Development Report
for Group Categorical I Exclusions
and Design Approval**

County: Pulaski
Local Public Agency: Pulaski County
Section Number: 12-00071-00-BR
Route: FAS 937

Project Number: 1FDA(058) Project Length: 900' (0.170 mi.)

Street/Road Name: CH 2 (Tick Ridge Road)

Termini: Approximately 415' west of the structure centerline to approximately 485' east of the structure centerline.

For Township or Road District bridge projects: The County Engineer certifies that the project design speed exceeds the minimum design speed recommended for this classification of roadway as provided in the BLRS Manual in order to prevent a deficient NBIS rating for approach roadway alignment appraisal. All elements have been designed to the chosen design speed unless noted otherwise in Section 2(e) and/or the attached BLR 22120.

County Engineer Date

Categorical Exclusion and Design Approval Recommended

Burl Watts 7-22-2021
Local Agency Date

Categorical Exclusion Statement

This project will not have any significant impacts on the environment, or involve any unusual circumstances, therefore, it is a Categorical Exclusion I.

Categorical Exclusion and Design Approval

Kevin Roberts 8-12-21
Regional Engineer Date



1. LOCATION AND EXISTING CONDITIONS

a. Location (attach location map to supplement narrative description)

See EXHIBITS A1, A2 and A3 (Location Maps).

The project is located approximately 4 miles southeast of Grand Chain, Illinois where CH 2 (Tick Ridge Road) crosses over Post Creek Cut-Off.

b. Description of Existing Facility - Give narrative description, including such items as width of travel, parking and turn lanes, sidewalks, alignment, traffic control devices, utilities, jurisdiction, maintenance responsibility, drainage, terrain and current land use (including major public facilities and local landmarks). Attach existing typical sections showing roadway widths, bridge widths, ROW widths, sidewalk widths, guardrail, curb and gutter and surface types.

See EXHIBITS B (Roadway Typical Sections), C (Plan and Profile) and D (Existing Bridge Section).

The existing CH 2 (Tick Ridge Road) roadway facility is a Federal Aid Secondary route in Pulaski County that is functionally classified as a rural major collector. The roadway is a two-lane facility, approximately 20' wide at the approach to the structure and a shoulder-to-shoulder width varying from 22'-26'±, without sidewalks, parking lanes, turn lanes or curb and gutters.

The existing structure is SN 077-3000 consisting of a three-span continuous steel superstructure (approx. 85'-4" - 108'-10" - 85'-4" spans), with a non-composite 7" concrete deck and an overall length of 283'-0" b/b/bk abutments with a grade of approximately ±0.23% peaked at middle to drain and zero skew. The structure is on a tangent alignment with no traffic control being present within the project limits. The deck measures 25'-4"± out-to-out with a clear roadway width of 22'-0"±, and sits approximately 60' above the streambed. Timber guardrail is present on one end of the bridge and steel plate beam guardrail is present on the other. The bridge itself has concrete "Texas Rail" throughout.

A waterline and communications lines are present within the project limits. The waterline is owned by Fort Massac Water District, runs adjacent to the roadway, and is carried across the creek by the bridge from one embankment to the other. The buried communications lines are controlled by Frontier Communications, run adjacent to the roadway, and become aerial to cross the creek.

The jurisdiction and maintenance responsibilities for this bridge fall to Pulaski County. The primary drainage in the area is controlled by drains located on the bridge deck along with concrete ditches adjacent to the road near the bridge draining onto concrete slope walls under the bridge. The terrain adjacent to the bridge is wooded and there is a steep elevation drop from the roadway down to the stream below. West of the creek are farm fields while to the northeast there is a grassy field and to the southeast is a residential yard.

c. Traffic Data

Current ADT: 550 % trucks: 5

Will 80,000 trucks be legally permitted on this route? Yes No

Design Year: 2041 ADT: 725 DHV: _____ % trucks: 5

d. **Structures** - Identify location within the proposed improvement of all structures on attached location map. Attach a copy of the Structure Master Report for all structures within the project limits. Attach a copy of the Bridge Condition Report or the Bridge Deck Resurfacing approval letter for structures to be replaced, rehabilitated, or resurfaced.

See EXHIBITS A1-A3 (Location Maps), E (Structure Master Report) and G (Bridge Condition Report Approval Letter).

e. **Railroads** - Identify location of all railroad crossings on attached location map and complete the following:

Railroad Name	No. and Type of Tracks (Main or Switching)	Type of Warning Devices*	No. of Trains Per Day	Railroad Width of Crossing at Rt. Angles
N/A				

*Include a sketch showing location of railroad protective devices from the edge of roadway and to the nearest track.

f. **Contiguous Sections** - Describe the existing typical sections at each end of the proposed improvement including number of travel lanes, turning lanes and parking lanes, lane widths and roadway width (f of curbs or e-e of shoulders), and sidewalk width.

The existing roadway typical section at the project termini consists of two 10± travel lanes with HMA surface that is approximately 20' wide with earth shoulders and a total out-to-out of shoulder width of approximately 26±. No turning lanes, parking lanes, or sidewalks are present.

2. Proposed Improvement

a. Discuss the purpose and need of the project:

See EXHIBITS C (Plan and Profile) and E (Structure Master Report)

The purpose of this project is to replace an aging, structurally deficient bridge built in 1938 with an unknown design load. The superstructure has been rated in poor condition with advanced deterioration, contributing to a sufficiency rating of 4.1.5. The Inventory and Operating Ratings are 0.620 and 1.330, respectively.

b. What design guidelines will be used for the proposed improvement? (Check One)

- Rural (BLRS Manual Chapter 32)
- Urban (BLRS Manual Chapter 32)
- Suburban (BLRS Manual Chapter 32)
- 3R Guidelines (BLRS Manual Chapter 33)
- Bicycle Guidelines (BLRS Manual Chapter 42)
- Pedestrian Guidelines
- Other:

Functional Classification: Arterial Collector Local Road Other _____

Terrain: Level Rolling

Regulatory or Posted Speed Limit: 55 _____ Design Speed: 50 _____

- c. Describe type of work to be accomplished by the improvement. Discussion should include width of proposed travel, parking, bicycle and turning lanes, sidewalks, shared-use paths, guardrail, traffic control devices, drainage items (including storm sewer outfalls), alignment changes, railroad work, utility adjustments, intersection improvements, side slopes and clear zones. Specify the e_max for horizontal curves. Attach typical sections, plan and profile sheets, and intersection design studies when applicable.

See EXHIBITS B (Roadway Typical Sections), C (Plan and Profile) and D (Proposed Bridge Section and TS&L)

The existing three-span steel structure (S.N. 077-3000) will be replaced with proposed S.N. 077-3145. The new bridge will be a three-span, 54" precast prestressed concrete I-beam bridge measuring 274' back-to-back of abutments, built inside of the existing. The proposed 30'-10" wide deck will provide a 28'-0" clear bridge width between the 39" tall concrete parapets and support two 11'-0" lanes with 3'-0" shoulders across the bridge. Precast bridge approach slabs will be topped with a nominal 5" concrete wearing surface. Type 6 and Type 1 (Special) Traffic Barrier Terminals will be attached to the concrete parapets at each corner of the bridge.

Proposed work will include 626' of roadway improvements for a total project length of 900'. The existing horizontal alignment will be maintained while the vertical profile will be improved by the addition of a crest vertical curve across the bridge to promote drainage. The profile will be raised approximately 2'-9"± at the new bridge abutments.

The proposed typical roadway section will consist of a 26'-0" HMA surface with 2'-0" aggregate shoulders typical beyond the bridge (3'-0" HMA shoulders in front of the guardrail near the bridge) providing two 11'-0" HMA lanes and 4'-0" shoulders (2'-0" HMA and 2'-0" aggregate) typical (3'-0" shoulders at the bridge), tapering out to match the existing roadway and turf (earth) shoulders at the end of the project. The travel lanes are narrower than HMA surface to allow the extra HMA to be considered as part of a combined shoulder layout, which is above the policy shoulder requirements. The extra pavement width was selected to better accommodate large vehicles travelling on these routes and minimize edge raveling. No parking, bicycle, or turning lanes will be present in these improvements nor will there be sidewalks or shared-use paths.

No traffic control devices will be used within the project limits.

Proposed front slopes will be 1:4 (V:H) typical with a transition to 1:2 behind the concrete barrier/guardrail near the structure, and some 1:3 areas to minimize impacts. The proposed back slopes will be 1:3 typical throughout the project length. Clear zone throughout the project is provided through the 1:4 and 1:3 typical side slopes, ditches, and guardrail near the structure.

Drainage ditches will be reshaped along most of the roadway alignment while maintaining the drainage similar to existing patterns. The across-road culvert west of the bridge will be replaced. Generally, the ditches will have a maximum of 1:3 side slopes with a 2'-0" flat bottom.

Water and communications utilities are present within the project limits. The waterline is owned by Fort Massac Water District, runs adjacent to the roadway, is carried across the creek by the bridge, and will have to be shut down during construction. The Water District has determined that boring a waterline is cost-prohibitive due to the proximity of rock and requested that a new waterline be supported on the new bridge and placed in conjunction with this project but at its own expense. The buried communications lines are controlled by Frontier Communications, run adjacent to the roadway, and become aerial to cross the creek. The aerial line will have to be avoided by the contractor while the poles and buried lines will have to possibly be adjusted during construction.

- d. Discuss items affecting improvement such as hazardous mailbox supports, parking and truck restrictions, mail delivery from traffic lanes, justification (including warrants) for multi-way stop signs, traffic signals and other traffic control and railroad protective devices, stage construction, nearby airports, and additional lighting:

No mailboxes are located within the project limits.

The roadway will be closed to through traffic during construction and typical stage construction will not be required. A marked detour route will be provided around the project and bridge closure.

e. Identify each aspect to be constructed at less than the design guidelines and provide a clear description of required design variances and appropriate justification. (BLRS Manual Section 27-7). If a design variance is required, include a copy of the approved BLR 22120 form as an attachment.
None per 3R policy.

f. Current estimated cost of proposed improvement? \$ 2,800,000 See EXHIBIT P (Preliminary EOC)

g. Analyze the need for accommodating pedestrians, bicyclists and the handicapped. When applicable, describe the facilities to be provided for pedestrians and bicyclists. Discuss the ADA accessibility and maximum longitudinal grade of these facilities. (BLRS Manual Chapter 41)

Special facilities for pedestrians, bicyclists and the handicapped are not required at this location. The improved roadway and bridge surface will be adequate to handle the occasional pedestrian and bicyclist. Handicapped traffic is not anticipated on this bridge due in part to its rural location.

Sidewalks/Shared-Use Paths:

Maximum 2% crosslope: Yes No Not Applicable

ADA ramps with detectable warnings at street intersections: Yes No Not Applicable
If no, provide justification.

h. Discuss any proposed improvements being considered in adjacent segments including the anticipated construction startup date of these improvements.
No improvements are planned for the surrounding area in the near future.

3. Crash Analysis (BLRS Manual Section 22-2.11(b)(9))

a. Summarize crash data for the past five years, including a spot map or a location map showing crash locations when possible. Detail the types of crashes and include collision diagrams, if possible, especially at cluster sites. Give the source of this data.

A search of accident reports by the County found no reported crashes between 1/1/2015 and 12/31/2019 within the project area.

b. Analyze available crash data including results of field check. Discussion should include high crash locations, critical wet weather sites, and other crash patterns. If the data is inconclusive, make a statement to that effect.
Since there have been no reported accidents, there is no data to analyze.

c. Describe how the proposed project will address any crash issues.
N/A

4. Right-of-Way

- a. Describe the right-of-way taking, including the total acreage required for each of the following categories: ROW, permanent easements, temporary easements and temporary land use permits. Include the width of taking, number of property owners, acreage of right-of-way and/or easements, character of land, i.e., farm, residential, commercial or publicly owned properties, anticipated impacts to properties that remain, and location of any improvements with respect to required right-of-way. Discuss any impacts on setbacks required by zoning.

See EXHIBIT C (Plan and Profile).

Neither additional ROW nor temporary easement are required for this project.

- b. Are any residents, businesses or farms to be displaced?

Yes No

If yes, describe the number and type of displacements anticipated and mitigation that will be taken to provide relief for this impact on an attached sheet.

N/A

5. Prime Farmland (BLRS Manual Section 20-10)

- a. If the project requires more than 3 acres/mile (0.75 hectares/kilometers), 10 acres (4 hectares) for a non-linear improvement, or the project ROW is not contiguous to the existing ROW, contact the Illinois Department of Agriculture and attach results of the coordination and summarize the results below.

Neither additional ROW nor easements are required for the project. Therefore, coordination with the Illinois Department of Agriculture is not required.

- b. The project requires consultation with the Natural Resource Conservation Service, Form AD-1006 has been completed and submitted to the local office of NRCS. The completed AD-1006 form is attached.

The impact of this project on farmland conversion has been evaluated in accordance with the requirements of the US Natural Resources (NRCS). The project will cover 3 acres or less of farmland per mile (0.75 hectares or less of farmland per kilometer) and the conversion will not result in more than minor impacts. Accordingly, the project conforms to the general form AD-1006 prepared by NRCS. Therefore, further coordination with NRCS on this project will not be necessary.

6. **Floodplain Encroachment (BLRS Manual Section 20-7)**

Does the proposed work cross or encroach upon a 100-year floodplain, including a regulatory floodway?
 Yes No

If yes, summarize the location hydraulics study, regulatory floodway restrictions, the effect of any encroachment (including a comparison between existing and proposed conditions) and the effect of over-the-road flow on the proposed transportation facility. Attach any available floodplain maps.

See EXHIBITS F (PBDHR Approval), H (Waterway Information Sheet), and I (FEMA Firmette).

The hydraulic model shows the created head to be 0.1' for the 20-yr. Design Flood, 0.1' for the 100-yr. Base Flood and 0.1' for the 500-yr. Max. Calculated Flood. Backwater from the Ohio River was also considered in modeling as the crossing is approximately 0.6 miles north of the Ohio River. There will be no over-the-road flow within the project limits under design conditions up to the 500-yr. Max. Calculated Flood as the existing roadway is over 26' above the 1937 HWE shown on the Existing Plans. The proposed bridge low chord has been raised approximately 1' and gives 33'-2"± min. clearance (freeboard) above the 20-yr. Design High Water Elev. of 328.1.

7. **Phase I & II NPDES Storm Water Permit Requirements (BLRS Manual Section 7-4.01)**

Will the project involve soil disturbance of 1 acre (0.4 hectares) or more?
 Yes No

If yes, the project must comply with the Phase II NPDES Storm Water Permit Requirements.

8. **"404" Permit (BLRS Manual Section 7-4.02)**

Does this project involve waters regulated by Section 404?
 Yes No

If yes, what type of 404 permit is required? Nationwide Individual Regional None

Attach a copy of any 404 permit authorization and/or coordination letters with the Corps of Engineers. If an individual Section 404 permit is required, please notify the Illinois Department of Transportation district office before submitting the application.

See EXHIBIT J (USACE Nationwide Permit No. 14 Letter)

9. **Special Waste (BLRS Manual Section 20-12)**

a. Following the special waste assessment screening criteria shown on Figure 20-12A of the BLRS Manual, is Preliminary Environmental Site Assessment (PESA) required?
 Yes No

b. Is work being done on property in the name of the state or are contract plans being prepared by the state?
 Yes No

c. If a PESA is required for either state or local ROW, did the PESA results determine that the project has Recognized Environmental Conditions (REC's) for special waste?
 Yes No

If the PESA results determine that the project contains REC's, describe how the special waste is proposed to be handled (including if a Preliminary Site Investigation (PSI) is required).

10. Environmental Survey (BLRS Manual Section 20-2)

Whenever a project involves land acquisition (including easements), any in-stream work (including drainage structure run-around), is located within or adjacent to historic properties listed in (or eligible for) the National Register of Historic Places, a bridge on the historic list, is near wetlands, or known locations of threatened or endangered species, the Environmental Survey Request Form should be submitted early in the project development phase.

- a. Wild and Scenic Rivers - If this project crosses or affects a river on the National Wild and Scenic Rivers System or a river listed in the Nationwide Inventory of Rivers with potential for inclusion on the system, include coordination between the National Park Service and the Bureau of Design and Environment (BDE).
 Involvement No Involvement

- b. Wetlands - Does the proposed work impact the use of regulatory wetlands?
 Yes No See EXHIBIT K1 (Biological Clearance)

If yes, indicate how the wetlands will be migrated: Banking Accumulation On-site Other

- c. Archaeological and Historical Preservation Include results of coordination. Does the project impact an archaeological or historic preservation site?
 Yes No See EXHIBIT K2 (Cultural Clearance)

If yes, describe any required documents.

- d. Threatened or Endangered Species – Does the project impact any endangered species or plants?
 Involvement No Involvement

Include copy of biological resources memorandum or signoff by BDE and/or IDNR.

See EXHIBIT K1 (Biological Clearance)

Tree removal is restricted from April 1 to September 30 due to potential endangered bat habitats.

- e. Stream Modification and Wildlife Impacts - Include copies of any correspondence between BDE and IDNR or U.S. Fish and Wildlife Service. Attach copies of any additional coordination between local agency and IDNR or U.S. Fish and Wildlife Service whenever required as a result of biological review by BDE. Address any proposed mitigation measures.
 Involvement No Involvement See EXHIBIT K1 (Biological Clearance)

11. Section 4(f) Lands (BLRS Manual Section 20-3)

- a. Does this project require any right-of-way, including temporary construction easements, from a publicly owned park, recreational area, wildlife and waterfowl, or any historic site in or eligible for the National Register of Historic Places?
 Yes No

- b. If yes, what type of of the Section 4(f) involvement has been completed?

Section 4(f) de minimis Standard Section 4(f) Temporary Occupancy None

12. Air Quality (BLRS Manual Section 20-11) Check One:

- a. This project is in an attainment area.

Projects within a portion of a nonattainment area for which the Chicago Metropolitan Agency for Planning (CMAAP) is the MPO.

This project is included in the _____ (transportation plan) and in the Transportation Improvement Program (TIP), endorsed by the _____, the region's Metropolitan Planning Organization. The _____ (transportation plan) was found to conform by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) on _____.

The TIP was found to conform by FHWA on _____ and by FTA on _____.

- Projects within a nonattainment area served by a Metropolitan Planning Organization other than CMAP.

This project is included in the Long-Range Transportation Plan and in the _____ Transportation Improvement Program (TIP) endorsed by _____, the Metropolitan Planning Organization (MPO) for the region in which the project is located.

On _____ the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) determined that the Long-Range Transportation Plan conforms with the transportation-related provisions of the Clean Air Act Amendments of 1990. The FHWA and the FTA determined on _____ that the TIP conforms with the Clean Air Act Amendments. These findings were in accordance with 40 CFR Part 93, "Criteria and Procedures for Determining Conformity to State or Federal Implementation Plans of Transportation Plans, Programs, and projects Funded or Approved Under Title 23 USC or the Federal Transit Act."

The project's design concept and scope are consistent with the project information used for the TIP conformity analysis. Therefore, this project conforms to the existing State Implementation Plan and the transportation-related requirements of the 1990 Clean Air Act Amendments.

b. Mobile Source Air Toxics (See BDE PM 52-06)

This project will not result in any meaningful changes in traffic volumes, vehicle mix, location of the existing facility, or any other factor that would cause an increase in emissions relative to the no-build alternative. As such, FHWA has determined that this project will generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special Mobile Source Air Toxic concerns. Consequently, this effort is exempt from analysis for MSATs.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSATs to decline significantly over the next 20 years. Even after accounting for a 64 percent increase in VMT, FHWA predicts MSATs will decline in the range of 57 to 87 percent, from 2000 to 2020, based on regulations now in effect, even with a projected 64 percent increase in VMT. This will both reduce the background level of MSATs as well as the possibility of even minor MSAT emissions from this project.

c. Construction-related Particulate Matter

Demolition and construction activities can result in short-term increases in fugitive dust and equipment-related particulate emissions in and around the project area. (Equipment-related particulate emissions are usually insignificant when equipment is well maintained.) The potential air quality impacts will be short-term, occurring only when demolition and construction work is in progress and local conditions are appropriate.

The potential for fugitive dust emissions typically is associated with building demolition, ground clearing, site preparation, grading, stockpiling of materials, on-site movement of equipment, and transportation of materials. The potential is greatest during dry periods, periods of intense construction activity, and during high wind conditions.

The Department's *Standard Specifications for Road and Bridge Construction* include provisions on dust control. Under these provisions, dust and airborne dirt generated by construction activities will be controlled through dust control procedures or a specific dust control plan, when warranted. The contractor and the Department will meet to review the nature and extent of dust-generating activities and will cooperatively develop specific types of control techniques appropriate to the specific situation. Techniques that may warrant consideration include measures such as minimizing track-out of soil onto nearby publicly-traveled roads, reducing speed on unpaved roads, covering haul vehicles, and applying chemical dust suppressants or water to exposed surfaces, particularly those on which construction vehicles travel. With the application of appropriate measures to limit dust emissions during construction, this project will not cause any significant, short-term particulate matter air quality impacts.

d. **Project-level Hot Spot Analysis. Check One:**

- This project is in an attainment area and does not require a hot spot analysis.
- This project does not meet the definition of a project of air quality concern as defined in 40 CFR 93.123(b)(1).
Due to _____

it has been determined that the project will not cause or contribute to any new localized PM_{2.5} or PM₁₀ violations or increase the frequency or severity of any PM_{2.5} or PM₁₀ violations. USEPA has determined that such projects meet the Clean Air Act's requirements without any further Hot-Spot analysis.

- This project is in a non-attainment or maintenance area and is a project of air quality concern. Therefore, a qualitative hot spot analysis is required. See Attachment _____

e. **COSIM**

Are through lanes or auxiliary turn lanes being added with this project?

- Yes
- No

If yes, has a COSIM pre-screen analysis been completed?

- Yes
- No

If yes, pre-screen analysis is attached as Attachment _____

If no, explain why an analysis has not been performed. _____

If yes, did the COSIM pre-screen analysis pass or fail? Pass Fail

If the COSIM pre-screen analysis failed, a full COSIM analysis would be required.

13. **Noise (BLRS Manual Section 20-6)**

- The referenced project meets the criteria for a Type III project established in 23 CFR Part 772. Therefore, the proposed project requires no traffic noise analysis or abatement evaluation. Type III projects do not involve added capacity, construction of new through lanes, changes in the horizontal or vertical alignment of the roadway, or exposure of noise sensitive land uses to a new or existing highway noise source.

- Based on the traffic noise analysis and noise abatement evaluation conducted, highway traffic noise abatement measures are likely to be implemented based on preliminary design. The noise barriers determined to meet the feasible and reasonable criteria are identified on the attachment. If it subsequently develops during final design that constraints not foreseen in the preliminary design or public input substantially change, the abatement measures may need to be modified or removed from the project plans. A final decision of the installation of the abatement measure(s) will be made upon completion of the project's final design and the public involvement process.

If this project involves a new alignment, additional lanes, or involves a significant alignment change, attach a traffic noise analysis.

14. Work Zone Transportation Management Plans

Does the project intersect or follow a state route?

Yes No

Is the state or local route considered a significant route?

Yes No Not Applicable

If yes, describe how the Work Zone Transportation Management Plan is being implemented.

N/A

15. Complete Streets (BLRS Manual Chapter 10)

Does the project include the addition of a travel, turning, or bi-directional turn lane on a state highway?

Yes No

If yes, describe how the Complete Streets Law requiring accommodating bicyclists on a state route apply.

N/A

16. Maintenance of Traffic (BLRS Manual Section 22-2.11(b)(9))

Discuss how vehicle traffic and pedestrians will be accommodated during construction, including the impacts of any road and/or sidewalk closure. If the road will be closed, include information concerning location of alternate routes, their ability to handle the additional traffic (street width, number of traffic lanes, structural adequacy, etc.), and the amount of adverse travel. When a marked detour route will be provided, include coordination with appropriate agencies, a description of the adverse travel, and include a map showing the alternate routes or marked detour in the report.

See EXHIBITS O1 (Detour Map) and O2 & O3 (Detour Acceptance Memos)

The road will be closed to all through traffic during construction. No property or residents will be land locked. A marked detour route will be setup using Routes IL 37 and IL 169 along with Massac CH 12 (Boaz Road) to route Tick Ridge Road traffic around the closure, while allowing local traffic access. Some travelers may find an alternate (unmarked) detour route using Routes IL 37, IL 169, and US 45 along with Massac CH 5 (Joppa, Portland, and Grand Chain Roads) to be preferable. The routes are adequate to handle the additional traffic load. All emergency, school and postal services will be notified before construction begins. Field and private entrances will remain accessible, as needed, throughout the time of construction.

17. Public Involvement (BLRS Manual Chapter 21)

- a. Summarize public informational meetings, formal public hearings, property owner signoffs, council or board meetings, media coverage, and personal contact with public. Include copies of newspaper advertisements, letter to property owners, public comments, and documents showing all public comments have been addressed.

See EXHIBITS L1, L2 and L3 (Public Involvement)

Letters were sent to the adjacent property owners advising them of the project (See Exhibit L1). The County received one positive comment in response to the letters (See Exhibit L2).

Also, the project has been openly discussed at multiple County Board Meetings with support for replacing the bridge.

Project notification letters regarding the road closure and detour route were sent to area first responders, schools and the post office which were most likely to be affected by the closure (See Exhibit L3).

- b. Has any opposition been expressed toward the improvement?

Yes No

If yes, briefly discuss the type and extent of opposition.

- c. If yes, discuss how the opposition has been addressed with the property owners?

18. Coordination: LA-IDOT-FHWA (BLRS Manual Section 22-1.02)

Has there been any coordination meetings for this project? Yes No

If yes, list the date(s) of the coordination meeting(s) below and attach coordination meeting minutes in the report.
October 23, 2012 and September 10, 2019 - See EXHIBITS M1 & M2 (Coordination Meeting Minutes)

19. Other Coordination

Attach results.

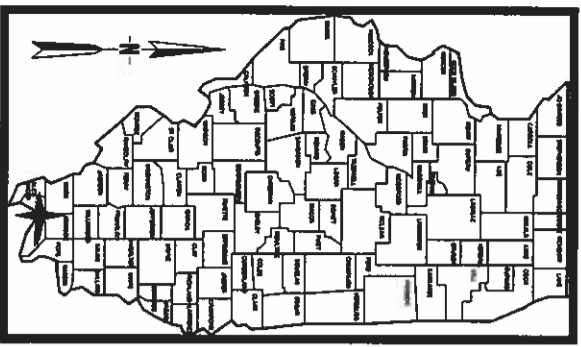
Coordination and discussions are ongoing with the Fort Massac Water District to accommodate a new waterline to be supported on the new bridge, constructed at the Water District's expense.

20. Summary of Commitments

- *A tree clearing restriction is present between April 1 and September 30 due to the possible presence of endangered bats.*
 - *Wetland and INAI sites are to be shown on the Plans and clearly marked in the field to avoid disturbance.*
 - *{Wetland} areas to be re-seeded shall utilize a Class 4 and 5 B wetland seed mix where feasible {if disturbed}.*
 - *See Exhibit K1 (Biological Clearance) where IDNR recommended some additional voluntary conservation measures for consideration during project design if feasible, regarding lighting and erosion control blanket.*
-

Summary of Attachments:

1. EXHIBIT A – Location Map and Functional Classification Map
 - a. EXHIBIT A1 – County Location Map
 - b. EXHIBIT A2 – Aerial Map
 - c. EXHIBIT A3 – Functional Classification Map
2. EXHIBIT B – Roadway Typical Sections
3. EXHIBIT C – Plan and Profile Sheets
4. EXHIBIT D – Structure Typical Section and TS&L Sheets
5. EXHIBIT E – Structure Master Report
6. EXHIBIT F – Preliminary Bridge Design and Hydraulic Report Approval Letter
7. EXHIBIT G – Bridge Condition Report Approval Letter
8. EXHIBIT H – Waterway Information Sheet
9. EXHIBIT I – FEMA Firmette
10. EXHIBIT J – Section 401/404 Permit Correspondence – USACE Nationwide Permit No. 14 Letter
11. EXHIBIT K – Environmental Clearances and Correspondence
 - a. EXHIBIT K-1 – Biological Clearance
 - b. EXHIBIT K-2 – Cultural Clearance
12. EXHIBIT L – Public Involvement
 - a. EXHIBIT L-1 – Letters to Adjacent Property Owners
 - b. EXHIBIT L-2 – County Memo (Email) Summary of Response
 - c. EXHIBIT L-3 – Project Notification Letters to Area First Responders, Schools, etc.
13. EXHIBIT M – Coordination Meeting Minutes
 - a. EXHIBIT M1 – 2012 Meeting
 - b. EXHIBIT M2 – 2019 Meeting
14. ~~EXHIBIT N – DELETED~~
15. EXHIBIT O – Detour Traffic
 - a. EXHIBIT O-1 – Detour Map
 - b. EXHIBIT O-2 – IDOT Detour Acceptance Memo
 - c. EXHIBIT O-3 – Massac County Detour Acceptance Letter
16. EXHIBIT P – Preliminary Estimate of Cost
17. EXHIBIT Q – Correspondence: CBLRS Approval of Draft PDR







Project Location
Section 12-00071-00-BR
Pulaski County
Tick Ridge Road (FAS 937 / CH 2) over Post Creek Cut-Off

Pulaski County

Bridge Replacement
Section 12-00071-00-BR

Legend

-  Campground
-  Jennifer's Trucking
-  Section 12-00071-00-BR
-  Tick Ridge Rd

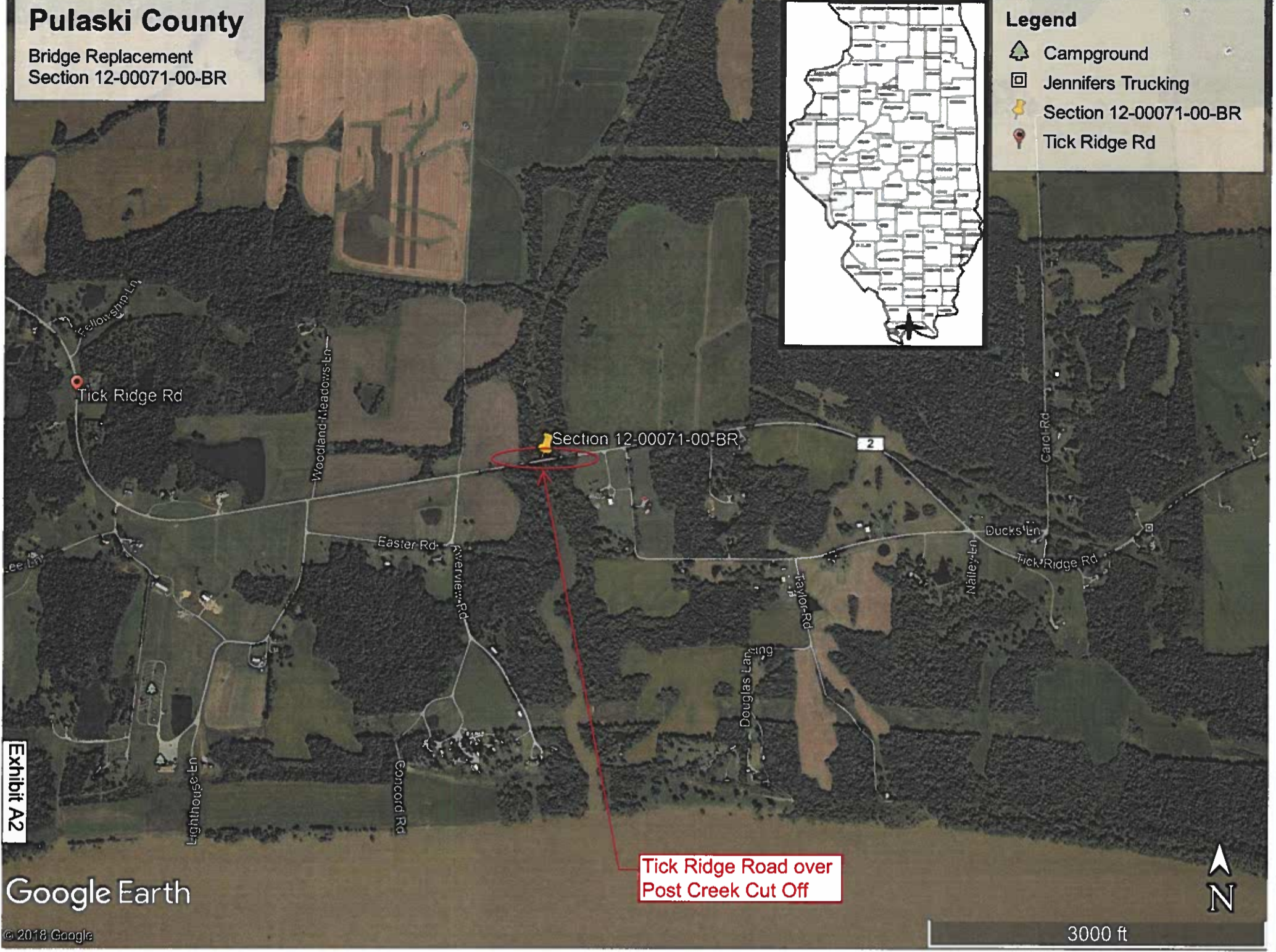


Exhibit A2

Google Earth

© 2018 Google

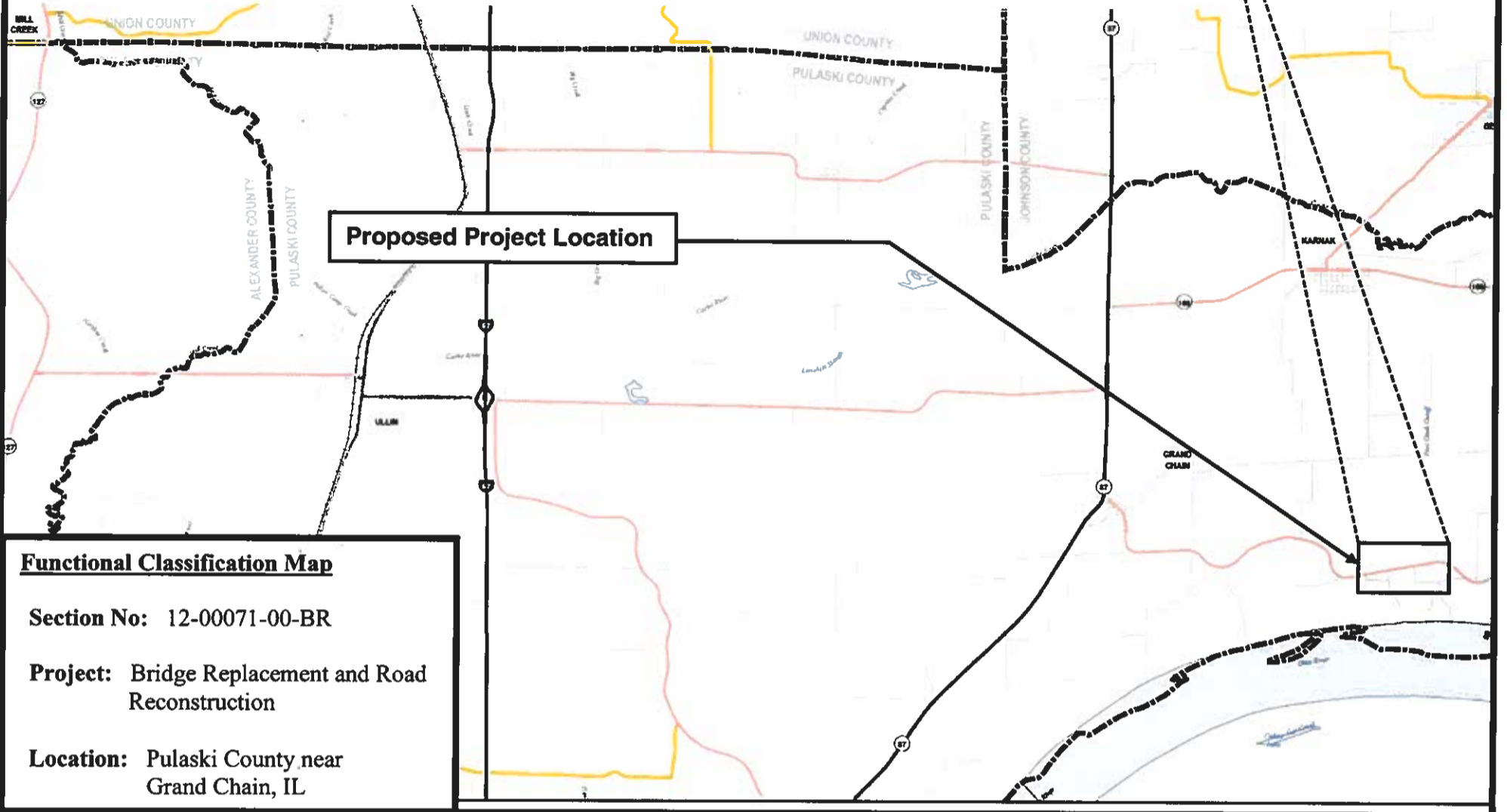
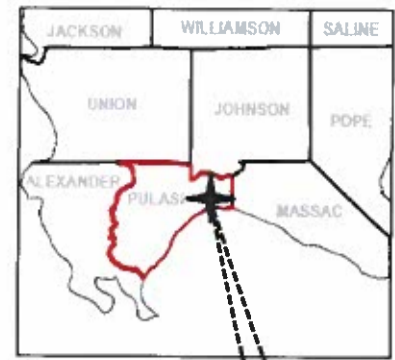
Tick Ridge Road over
Post Creek Cut Off

3000 ft

5-Year Classification

DATE MAP CREATED: 7/18/2019

- 1 Interstate
- 2 Freeway or Expressway
- 3 Other Principal Arterial
- 4 Minor Arterial
- 5 Major Collector
- 6 Minor Collector
- 7 Local Road or Street
- Interstate, U.S., State
- Railroad
- State Boundary
- County Boundary
- Urban Boundary
- River, Streams
- Lakes



Functional Classification Map

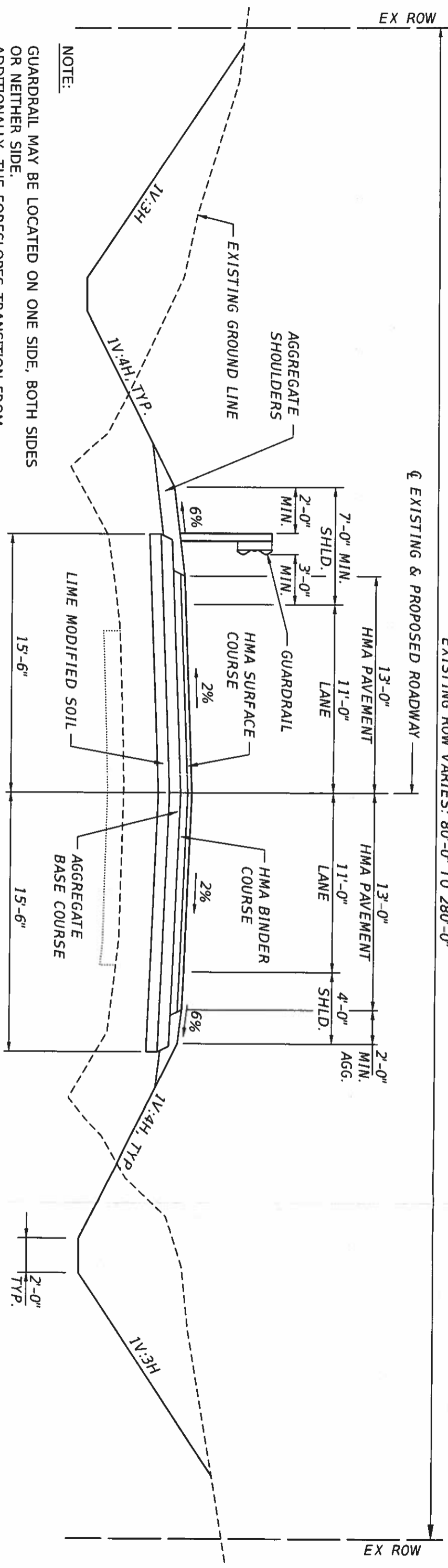
Section No: 12-00071-00-BR

Project: Bridge Replacement and Road Reconstruction

Location: Pulaski County near Grand Chain, IL

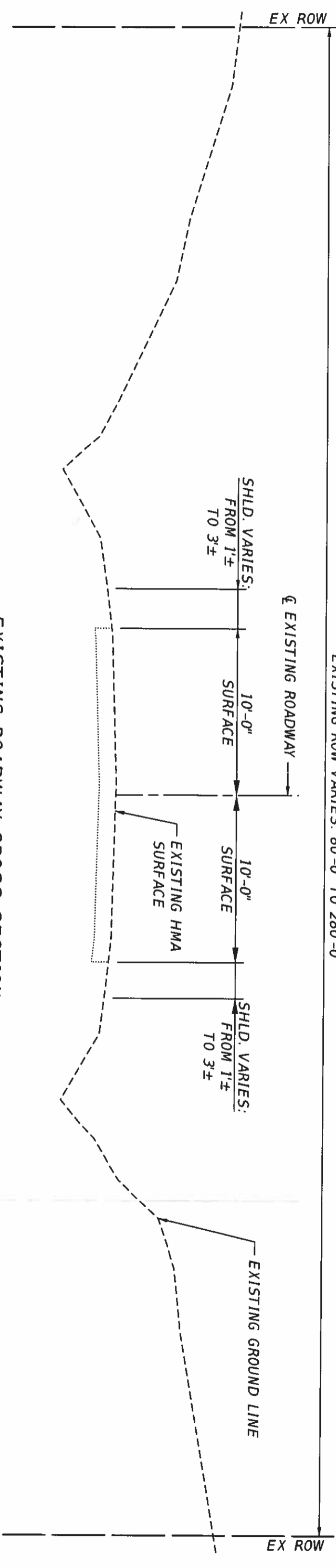
Exhibit A3

EXISTING ROW VARIES: 80'-0" TO 280'-0"



NOTE:
 GUARDRAIL MAY BE LOCATED ON ONE SIDE, BOTH SIDES OR NEITHER SIDE.
 ADDITIONALLY, THE FORESLOPES TRANSITION FROM 1V:3H TO 1V:2H AT THE BACK OF THE ABUTMENTS WHILE BEHIND GUARDRAIL/CONCRETE BARRIER. SEE PLAN AND PROFILE SHEET FOR EXACT LOCATIONS.
 SHOULDERS WILL TYPICALLY BE 4'-0", BUT WILL VARY FROM 4'-0" TO 7'-0" MIN. NEAR THE GUARDRAIL. SEE PLAN AND PROFILE SHEET FOR LIMITS OF SHOULDER WIDTH TRANSITIONS.

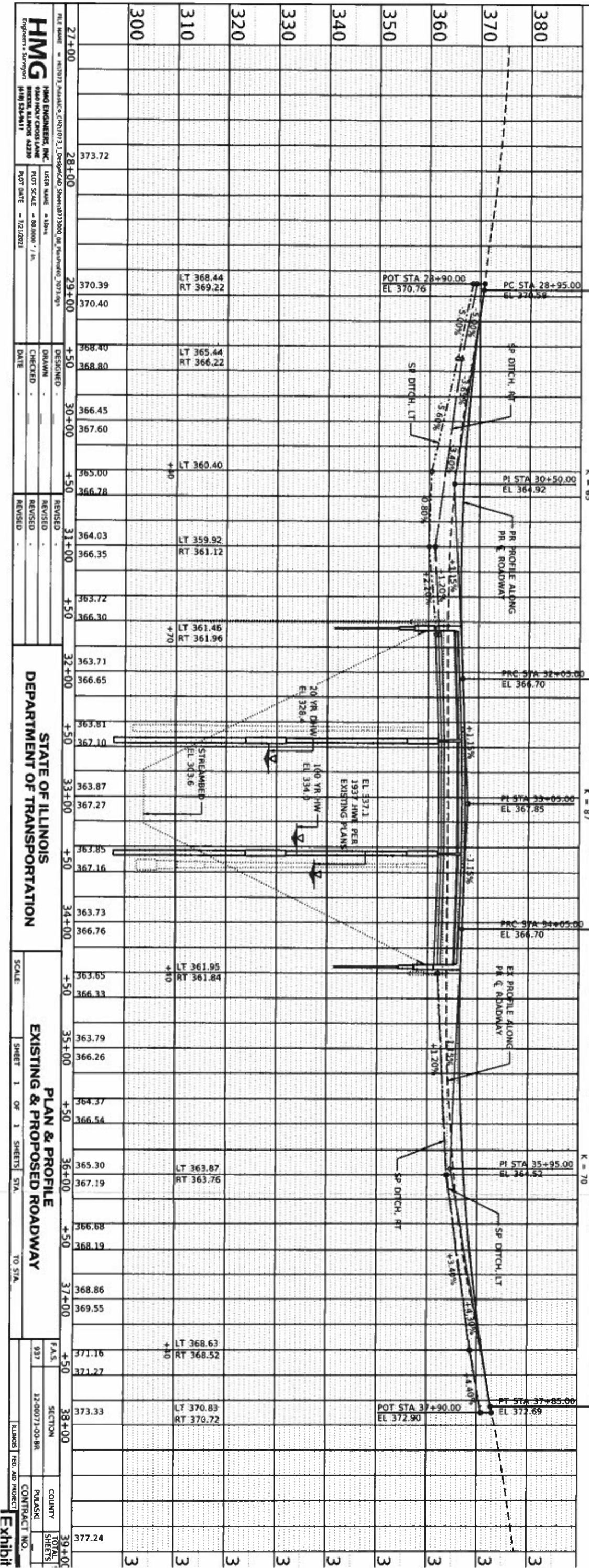
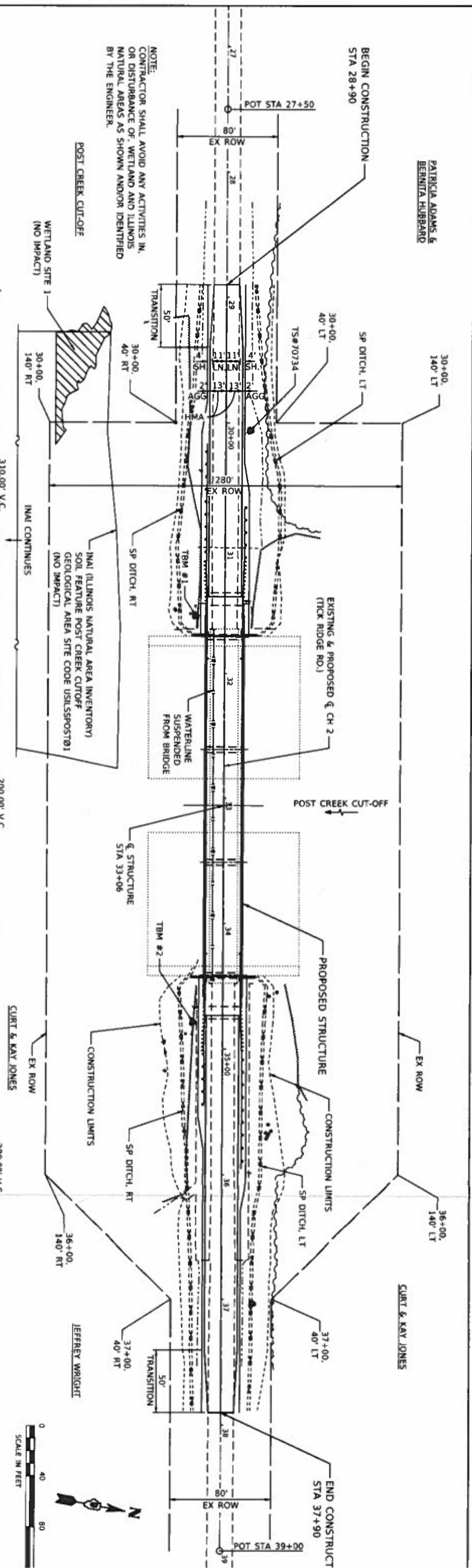
EXISTING ROW VARIES: 80'-0" TO 280'-0"



EXISTING ROADWAY CROSS SECTION

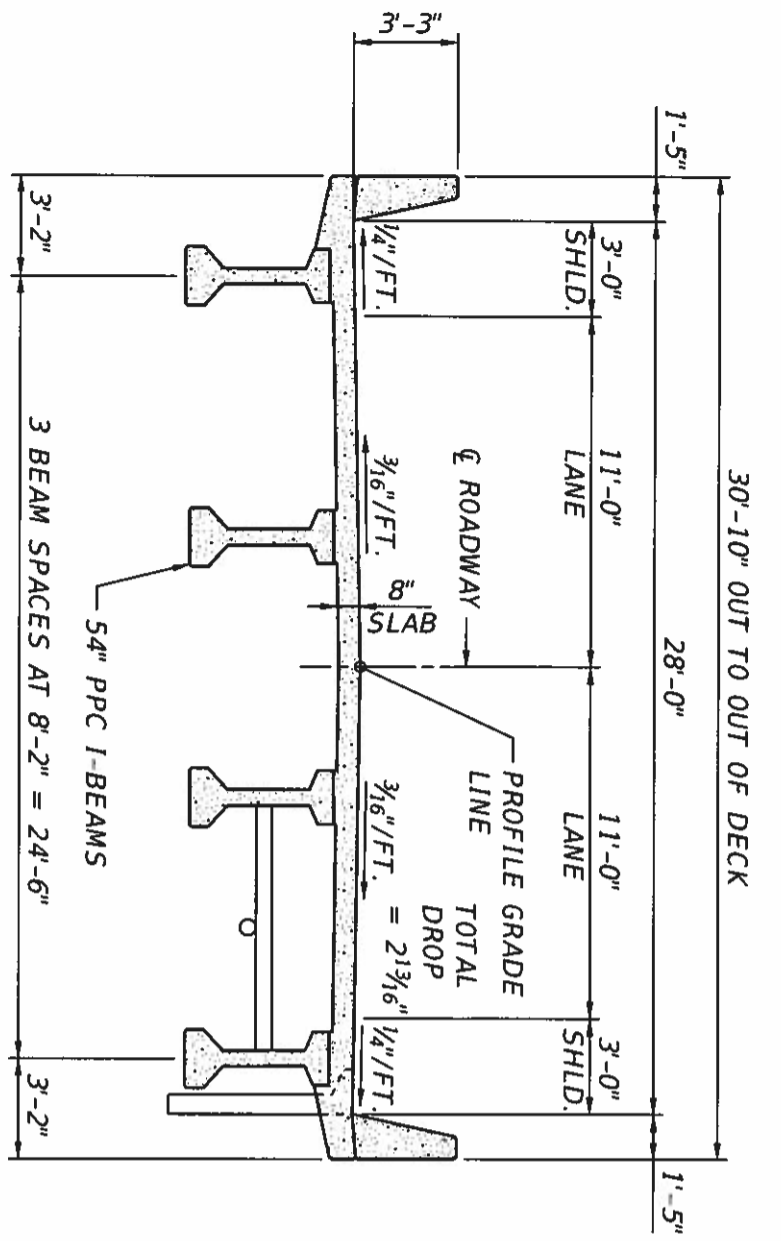
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNMENT CHECKED		
	RT. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	E.L.M. NOTED		
	STRUCTURE NOTATIONS CHECKED		

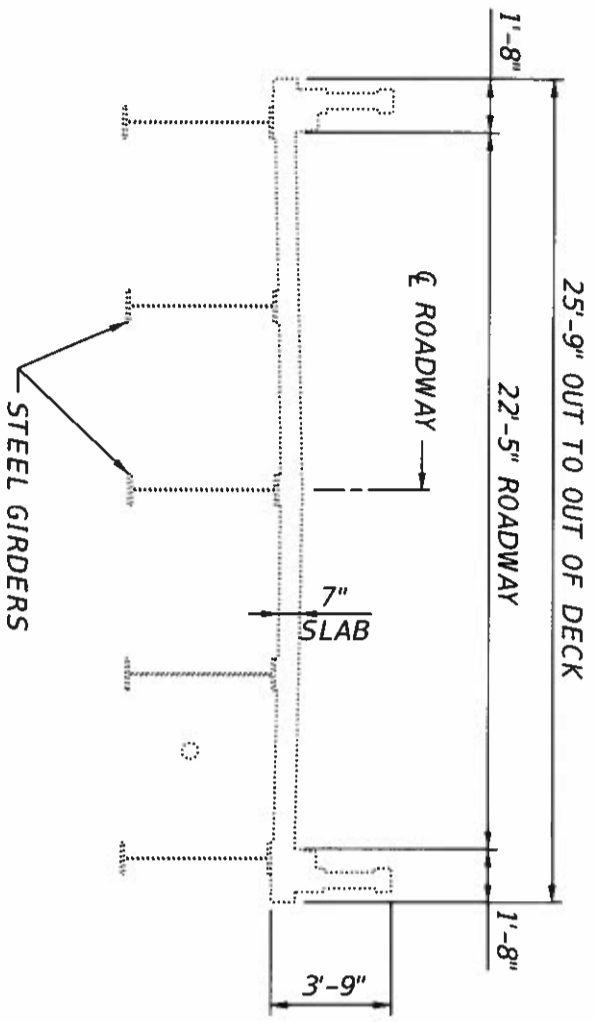


HMG ENGINEERS, INC.
 1500 N. CROSS LAKE
 DEPT. 1000
 DEPT. OF TRANSPORTATION
 STATE OF ILLINOIS
 EXISTING & PROPOSED ROADWAY
 PLAN & PROFILE
 SHEET 1 OF 1 SHEETS
 STA. TO STA.
 SCALE: 1" = 40'

Exhibit



PROPOSED BRIDGE CROSS SECTION



EXISTING BRIDGE CROSS SECTION

Exhibit D

BENCHMARK: TBM #1: RR Spike in Power Pole
Sta 31+54.04, 23.14' Rt
El. 364.03

TBM #2: RR Spike in Power Pole
Sta 34+78.49, 24.01' Rt
El. 365.19

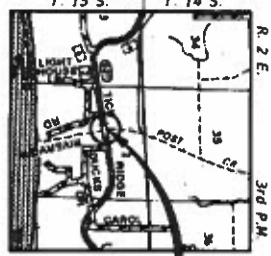
274'-0" Back to Back of Abutments

60' min.

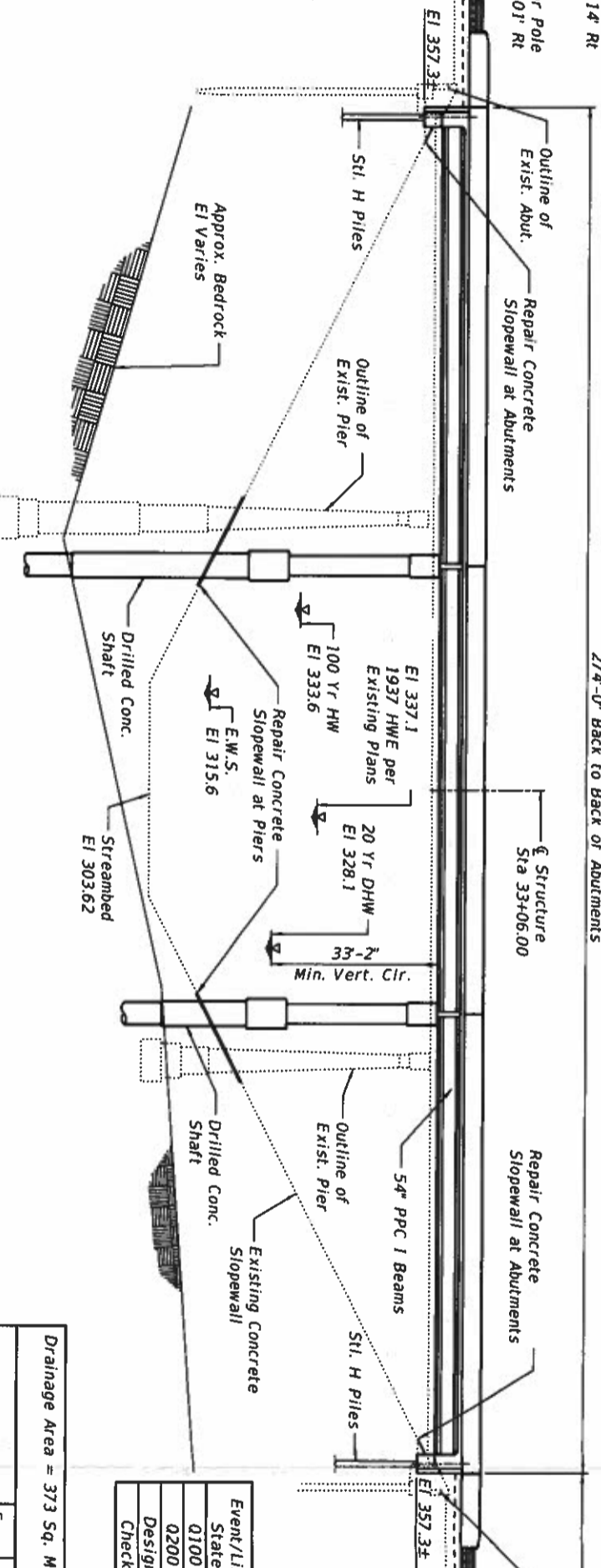
EXISTING STRUCTURE: SN 077-3000
Constructed in 1938, the existing structure consists of three span continuous steel superstructure with concrete deck spanning spill thru concrete abutments on concrete piers and two column concrete piers with tapered legs, one cross beam and spread footings without piles. The existing structure measures 283'-0" back to back of abutments and 25'-4"± out to out of the deck.

The existing roadway will be closed to traffic during the construction.

SALVAGE: The existing concrete stopwall shall remain.



LOCATION SKETCH



ELEVATION

ROADWAY CLASSIFICATION
Major Collector A.D.T. = 400-750
Current A.D.T. = 550 (2018)
Design A.D.T. = 725 (2041)
Design Speed = 40 mph

Drainage Area = 373 Sq. Mi.

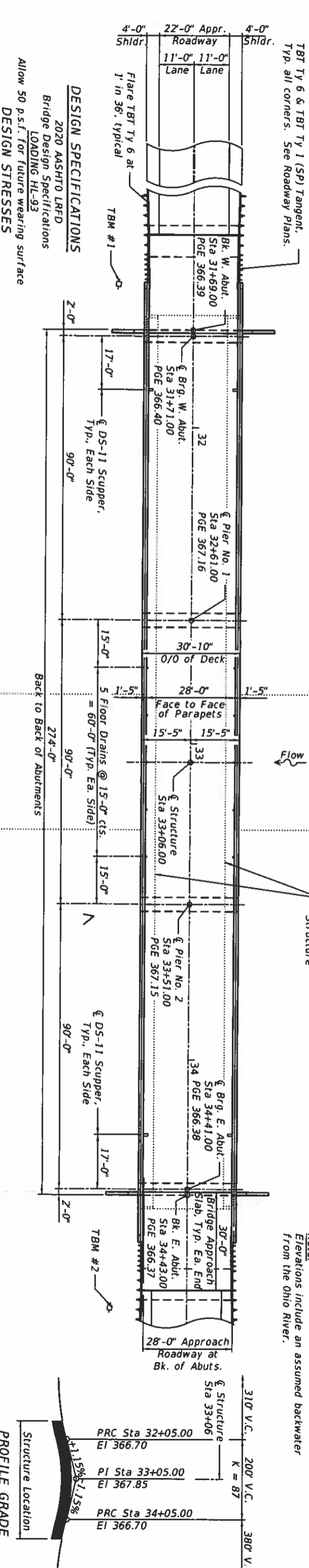
Flood Event	Freq. Yr.	C.F.S.	Opening Sq Ft	Nat. H.W.E.	Head - Ft	Headwater Elevation	Item
Design	20	16,370	2253	2180	0.1	328.2	328.
Base	100	22,000	3070	2980	0.1	333.7	333.
Scour Design Check	200	23,430	3200	3106	0.1	334.5	334.
Overtopping	N/A						
Max. Calc.	500	27,700	3412	3312	0.1	335.6	335.

Existing Overtopping EI = 363.52 at Sta 31+65
Proposed Overtopping EI = 366.01 at Sta 34+6

DESIGN SCOUR ELEVATION TABLE

WATERWAY INFORMATION

Event/Limit State	W. Abut.	Pier 1	Pier 2	E. Abut.	Item
Q100	302.61	302.61	306.0±	306.0±	113
Q200	302.42	302.42	306.0±	306.0±	
Design	357.3	302.61	306.0±	357.3	5
Check	357.3	302.42	306.0±	357.3	



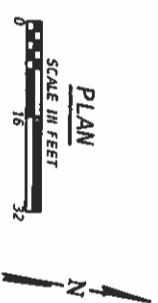
DESIGN SPECIFICATIONS
2020 AASHTO LRFD
Bridge Design Specifications
LOADING HL-93
Allow 50 p.s.f. for future wearing surface

DESIGN STRESSES

PRECAST PRESTRESSED UNITS

FIELD UNITS

SEISMIC DATA
Seismic Performance Zone (SPZ): 3
Design Spectral Acceleration at 1.0 sec. (S_{0.1}) = 0.414 g
Design Spectral Acceleration at 0.2 sec. (S_{0.2}) = 1.571 g
Soil Site Class = C



GENERAL PLAN
FAS 937 (CH 2 / TICK RIDGE F OVER POST CREEK CUT-OFF SECTION 12-00071-00-BR PULASKI COUNTY STATION 33+06.00 STRUCTURE NO. 077-3145

HMG ENGINEERS, INC.
1940 HOLY CROSS LANE
BEEBE, ILLINOIS 62320
Engineer: Dongyuan
618-524-9111

DESIGNED: []
DRAWN: []
CHECKED: []
DATE: []

REVISIONS:

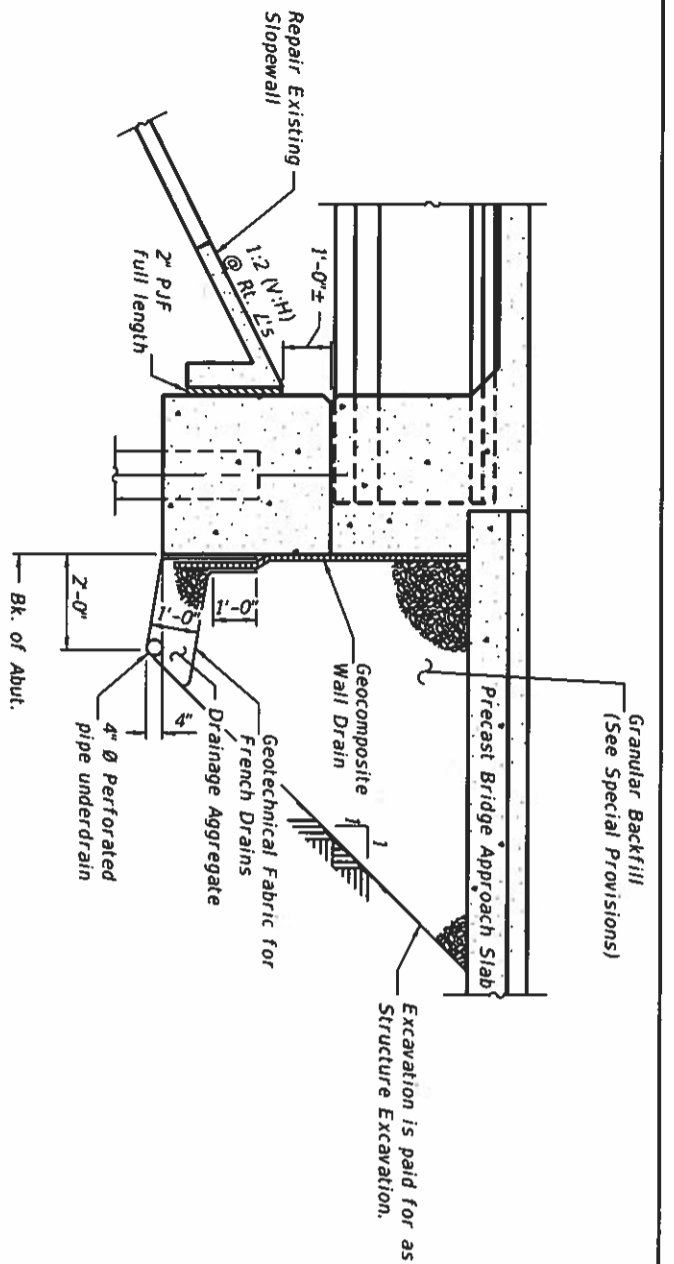
NO.	DESCRIPTION	DATE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

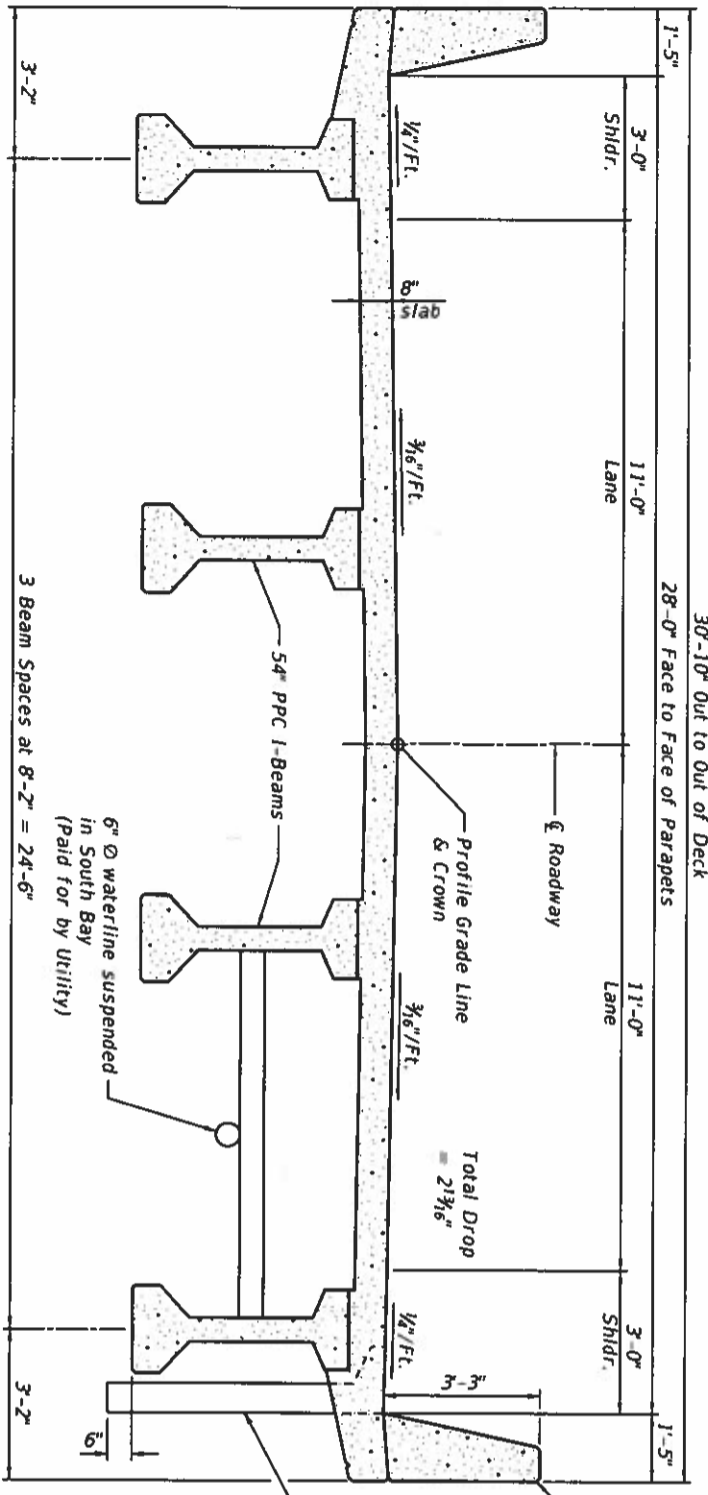
SCALE: SHEET 1 OF 2 SHEETS STA.

F.A.S. SECTION 12-00071-00-BR PULASKI COUNTY CONTRACT NO. 12-00071-00-BR

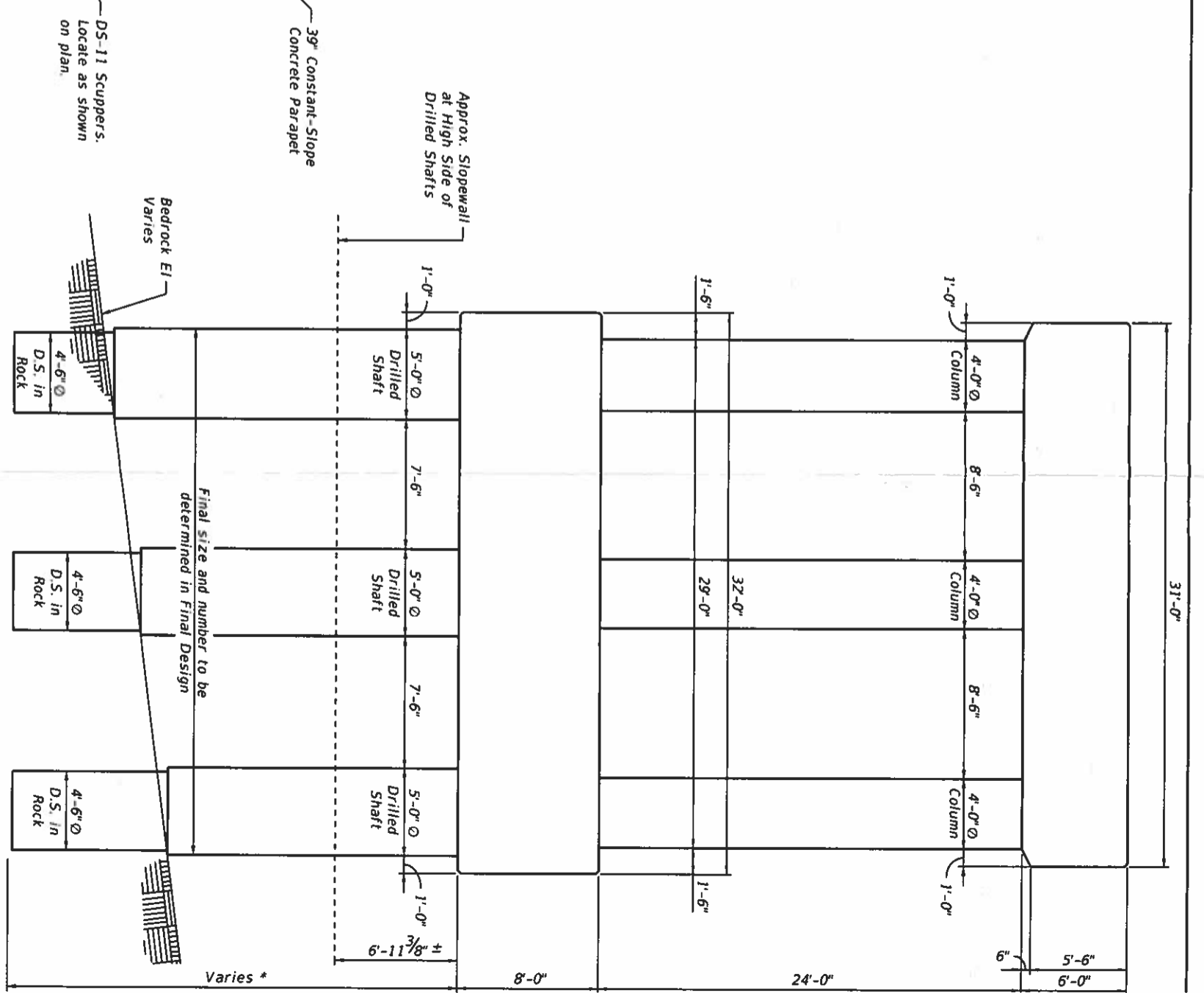
Exhibit



Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).



TYPICAL CROSS SECTION
(Looking East/Upstation)



* Note:
Contractor shall verify bedrock depth and quality after existing structure removal. Engineer of record shall then verify design before construction of drilled shafts.

PIER ELEVATION

GENERAL PLAN
FAS 937 (CH 2 / TICK RIDGE RIVER)
OVER POST CREEK CUT-OFF
SECTION 12-00071-00-BR
PULASKI COUNTY
STATION 33+06.00
STRUCTURE NO. 077-3145

<p>FILE NAME - K12073_Planetec_C4212073_1_Dwg\p\k\c\02_Sheet\0773100_1.dwg HMG ENGINEERS, INC. 9340 HOLY CROSS LANE BREESE BLVD. #220 ENGINEERS & ARCHITECTS (618) 524-4111</p>	<p>DESIGNED BY - DRAWN BY - CHECKED BY - DATE -</p>	<p>REVISIONS 1 2 3</p>	<p>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	<p>SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.</p>	<p>F.A.S. SECTION COUNTY TOTAL SHEETS 937 12-00071-00-BR PULASKI 5 ILLINOIS FED. AID PROJECT CONTRACT NO.</p>
--	--	---	--	--	---

Structure Number: 077-3000 District: 9

Inventory Data

Facility Carried:	FAS 937	Bridge Name:	POST CREEK OUT OFF	Sufficiency Rating:	41.5	Structure Length:	283.0
Feature Crossed:	POST CR CUT-OFF	Location:	4 MI E GRAND CHAIN	HBP Eligible:	Yes	AASHTO Bridge Length:	99.9
Bridge Remarks:				Replaced By:	077-3145	Length of Long Span:	109.0
Bridge Status:	1 OPEN - NO RESTRICT	StatusDate:	4/1/2003	Replaces:		Bridge Roadway Width:	22.0
Status Remarks:				Last Update Date:	03/30/2021	Appr Roadway Width:	20.0
Maint County:	077 PULASKI	Maint Township:	01 CO UNIT ROAD DIST	Parallel Structure:	None	Deck Width:	25.6
Maint Responsibility:	30 COUNTY		UNKNOWN	Multi-Level Structure Nbr:		Sidewalk Width Right:	0.0
Service On/Under:	1 HIGHWAY	/	5 WATERWAY	Skew Direction:	None	Sidewalk Width Left:	0.0
Reporting Agency:	3 COUNTY			Skew Angle:	0 D	Navigation Control:	0 No
Main Span Matl/Type:	3 STEEL	/	02 STRINGER/MULTI-BEAM/GIRDER	Structure Flared:	No	Navigation Horiz Clear:	0
Nbr Of Main Spans:	3	Nbr Of Approach Spans:	0	Historical Significance:	No	Navigation Vert Clear:	0
Approaches				Border Bridge State:		Culvert Fill Depth:	0.0
Near #1 Matl/Type:		/		Bdr State SN:		Number Culvert Cells:	0
Near #2 Matl/Type:		/		Bdr State % Responsibility:	0	Culvert Opening Area:	0.0
Far #1 Matl/Type:		/		Structural Steel Wt:	0	Culvert Cell Height:	0.00
Far #2 Matl/Type:		/		Substructure Material:		Culvert Cell Width:	0.00
Median Width/Type:	0 Ft / 0 None			Rated By:	2 IDOT	Rate Method:	6 LOAD FACTOR (LF) REP
Guardrail Type L/R:	0 None / 0 None			Inventory Rating:	0.620 (22)	Load Rating Date:	12/07/2017
Toll Facility Indicator:	0 No Toll			Operating Rating:	1.330 (47)	***Railroad Crossing Info***	
Latitude:	37.24018075	Longitude:	88.95978017	Design Load:	99 UNKNOWN	Crossing 1 Nbr:	
Deck Structure Type:	A CIP CON NRMLLY FORM	Deck Structure Thickness:	7.0	SD:	Y	FO:	N
Sidewalks Under Structure:	0 None					RR Lateral Underclear:	0.0
						RR Vertical Underclear:	0 Ft 0 In

Key Route On Data

Key Route Nbr:	FEDERAL-AID SECONDARY	0937	Station:	4.4100
Appurtenances	Main Route	00000	Segment:	
Inventory County:	077 PULASKI		Linked:	Y
Township/Road Dist:	01 CO UNIT ROAD DIST		Natl. Hwy System:	Not on NHS
Municipality:	0000		Inventory Direction:	
Urban Area:	None		Curr AADT Yr/Count:	2018 / 550
Functional Class:	5 MAJOR COLLECTOR		Est Truck Percentage:	5 %
** CLEARANCES **		South/East	North/West	Number Of Lanes:
Max Rdwy Width:	22.0			2
Horizontal:	22.5	0.0		One Or Two Way:
Min Vertical:	99Ft 11in	00Ft 00in		2 Two-Way
10 Ft Vertical:	99Ft 11in	00Ft 00in		Bypass Length:
Lateral:				4
				Future AADT Yr/Cnt:
				2032 / 665
				Designated Truck Rte:
				NONE
				Special Systems:
				No

Key Route Under Data

Station:	
Segment:	
Linked:	
Natl. Hwy System:	
Inventory Direction:	
Curr AADT Yr/Count:	/
Est Truck Percentage:	%
Number Of Lanes:	
One Or Two Way:	
Bypass Length:	
Future AADT Yr/Cnt:	/
Designated Truck Rte:	
Special Systems:	

*** Marked Route On Data ***

Route #	Designation	Kind	Number
Route #1:	1 Mainline	4 FAS, CH, or TR's Unmarked	
Route #2:	1 Mainline		
Route #3:	1 Mainline		

*** Marked Route Under Data ***

Designation	Kind	Number

Exhibit E

Structure Number: 077-3000 District: 9

Data Related to Inspection Information

***Inspection Intervals ***
 Routine NBIS: 24 MOS Underwater: 0 MOS One Truck At A Time: 0 Tons
 Fracture Critical: 0 MOS Special: N Tons Single Unit Vehicles: Tons
 Bridge Posting Level: 5 No Posting Required

Inspection/Appraisal Information

Inspection Date: 12/03/2020 Inspection Temperature: 48 Deg F Insp by (Name): Watson, James B
 Deck: 5 FAIR CONDITION - MINOR SECTION LOSS, CRACKS Insp by (Name):
 Superstructure: 4 POOR CONDITION - ADVANCED DETERIORATION Utilities Attached:
 Substructure: 5 FAIR CONDITION - MINOR SECTION LOSS, CRACKS
 Culvert: N NOT APPLICABLE
 Channel and Protection: 6 SATISFACTORY CONDITION - MINOR DETERIORATION Deck Wearing Surf: K GRAVEL MACADAM
 Structural Evaluation: 4 MINIMUM ADEQUACY TO BE LEFT IN PLACE Deck Membrane: F NONE
 Deck Geometry: 4 MINIMUM ADEQUACY TO BE LEFT IN PLACE Deck Protection: J NONE
 Underclearance-Vert/Lat: N NOT APPLICABLE Total Deck Thick: 7.2
 Waterway Adequacy: 9 SUPERIOR TO PRESENT DESIRABLE CRITERIA Last Paint Date:
 Approach Roadway Align: 7 BETTER THAN PRESENT MINIMUM CRITERIA Inspection Remarks:
 Bridge Railing Appraisal: 2 Doesn't Meet Standards
 Approach Guardrail: 222 Not Acceptable Not Acceptable
 Pier Navig Protection: N N/A

Underwater Inspection/Appraisal Information

Inspection Date: Inspection Method: Inspection Rating:
 Temperature: Inspected By: Appraisal Rating:
 Inspected By: Appraisal Rating:
 Inspection Remarks:

Scour Critical Information

Rating: 8 CALCULATED SCOUR ABOVE FOOTING Evaluation Method: B Rational Analysis
 Analysis Date: 05/15/1996 Analysis By: J PLEMON

Construction Information

Year: 1938 Original Reconstructed
 Route: SA 2 Sta: 33+06 Sta:
 Section Nbr: 12 B
 Contract Nbr:
 Fed Aid Pr #: FAS 561A000000
 Built By: 1 I.D.O.T.

Proposed Improvement

Cost Estimate Year: Length: *** Costs in Dollars ***
 Type of Work: Bridge Cost:
 Done By: Roadway Cost:
 Remarks: Total Project Cost:

Miscellaneous

Fracture Critical Members: No
 Microfilm Data Recorded: No



Illinois Department of Transportation
Office of Highways Project Implementation / Bureau of Bridges & Structures
2300 South Dirksen Parkway / Springfield, Illinois / 62764

March 5, 2021

Illinois Special Bridge Program
Pulaski County
Section 12-00071-00-BR

SN 077-3145

FAS 937 (CH 2 – Tick Ridge Road) over Post Creek Cut-Off

Mr. James B. Watson
County Engineer
Pulaski County
P.O. Box 97
Villa Ridge, Illinois 62996

Dear Mr. Watson:

The Preliminary Bridge Design and Hydraulic Report, dated January 27, 2021, for the above-designated bridge replacement project is satisfactory based on your consultant's certification and on having met the requirements for Statewide Permit #2. The preliminary bridge design is hereby approved.

The final structural plans and specifications for this project may be accepted by the District based on the Structural Engineer's seal, certification and signature per BLRS Manual Section 23-7.02.1, and on a **sealed structure load rating** submittal as described in BLRS Circular Letter 2017-16 and Structural Services Manual Section 4.2.2.

A PDF of the approved report is being emailed to you for your use and we will retain a copy for our files. If you have any questions, contact Matt Humke at (217) 782-5929 or matt.humke@illinois.gov.

Sincerely,

D. Carl Puzey
Engineer of Bridges and Structures

A handwritten signature in black ink, appearing to read "Tim A. Armbricht".

By: Timothy A. Armbricht
Engineer of Structural Services

MH/KK0773145-20210305

cc: HMG Engineers, Inc. / Attn: Larry D. Gowler
Keith W. Roberts, District 9 / Attn: Doug J. Helfrich

Exhibit F



Illinois Department of Transportation
Office of Highways Project Implementation / Bureau of Bridges & Structures
2300 South Dirksen Parkway / Springfield, Illinois / 62764

March 5, 2021

Illinois Special Bridge Program
Pulaski County
Section 12-00071-00-BR

SN 077-3000

FAS 937 (CH 2 – Tick Ridge Road) over Post Creek Cut-Off

Mr. James B. Watson
County Engineer
Pulaski County
P.O. Box 97
Villa Ridge, Illinois 62996

Dear Mr. Watson:

The Bridge Condition Report, dated January 2021, for the above-designated bridge replacement project is satisfactory and is hereby approved.

Approval of the project is contingent on approval by others of the proposed geometry, obtaining environmental signoffs, any required historic structure coordination and other approvals required by statutes or the policies of the Department.

A PDF of the approved report is being emailed to you for your use and we will retain a copy for our files. If you have any questions, contact Matt Humke at (217) 782-5929 or matt.humke@illinois.gov.

Sincerely,

D. Carl Puzey
Engineer of Bridges and Structures

A handwritten signature in blue ink that reads "Timothy A. Armbricht".

By: Timothy A. Armbricht
Engineer of Structural Services

MH/Kkt0773000-20210305
cc: HMG Engineers, Inc. / Attn: Larry D. Gowler
Keith W. Roberts, District 9 / Attn: Doug J. Helfrich

Exhibit G

Route: CH 2 (Tick Ridge Road)
Section: 12-00071-00-BR
County: Pulaski
Prepared by: KMM Date: 1/26/2021

Existing SN: 077-3000
Proposed SN: 077-3145
Waterway: Post Creek Cut-Off
Checked by: LDG Date: 1/26/2021

Flood Event		Freq. Yr.	Discharge Ft ³ /s	Waterway Opening - ft ²		Natural H.W.E. ft.	Head - ft.		Headwater Elevation ft.	
				Existing	Proposed		Existing	Proposed	Existing	Proposed
		10	14,300	1912	1847	325.4	0	0.1	325.4	325.5
Design		20	16,370	2253	2180	328.1	0.1	0.1	328.2	328.2
Base		100	22,000	3070	2980	333.6	0.1	0.1	333.7	333.7
Scour Design Check		200	23,430	3200	3106	334.4	0.1	0.1	334.5	334.5
Overtop Existing		>500								
Overtop Proposed		>500								
Max. Calc.		500	27,700	3412	3312	335.5	0.1	0.1	335.6	335.6

10 YEAR VELOCITY THROUGH EXISTING BRIDGE = 7.5 ft/s

10 YEAR VELOCITY THROUGH PROPOSED BRIDGE = 7.8 ft/s

ALL-TIME H.W.E. & DATE: EL 337.1 - 1937
Scope of Work:
EXISTING STRUCTURE

TYPE: CIP Concrete Deck on Steel Girder
LENGTH: 283'-0" Bk-Bk Abutments
SPANS: 3
LOW BEAM: 360.07
SKEW: 0
LOW E.O.P.: 363.43

PROPOSED STRUCTURE

TYPE: 8" Concrete Deck on 54" PPC I Beams
LENGTH: 274'-0" Bk-Bk Abutments
SPANS: 3
LOW BEAM: 361.25
SKEW: 0
LOW E.O.P.: 366.02

NOTE: ABOVE WATERWAY INFO INCLUDES CONCURRENT FLOODING BACKWATER ELEVATIONS FROM THE OHIO RIVER.

Exhibit H



Route: CH 2 (Tick Ridge Road)
 Section: 12-00071-00-BR
 County: Pulaski
 Prepared by: KMM Date: 1/26/2021

Existing SN: 077-3000
 Proposed SN: 077-3145
 Waterway: Post Creek Cut-Off
 Checked by: LDG Date: 1/26/2021

Flood Event	Freq. Yr.	Discharge Ft ³ /s	Waterway Opening - ft ²		Natural H.W.E. ft.	Head - ft.		Headwater Elevation ft.	
			Existing	Proposed		Existing	Proposed	Existing	Proposed
Drainage Area = 373 sq. mi.			Existing Overtopping Elev. = 363.52			at Sta. 31+69			
			Proposed Overtopping Elev. = 366.01			at Sta. 34+85			
	10	14,300	1582	1529	321.2	0	0	321.2	321.2
Design	20	16,370	1734	1678	322.3	0	0	322.3	322.3
Base	100	22,000	2143	2076	325.1	0	0.1	325.1	325.2
Scour Design Check	200	23,430	2247	2177	325.8	0	0.1	325.8	325.9
Overtop Existing	>500								
Overtop Proposed	>500								
Max. Calc.	500	27,700	2559	2481	327.6	0.1	0.2	327.7	327.8

10 YEAR VELOCITY THROUGH EXISTING BRIDGE = 9.1 ft/s

10 YEAR VELOCITY THROUGH PROPOSED BRIDGE = 9.4 ft/s

ALL-TIME H.W.E. & DATE: EL 337.1 - 1937

Scope of Work:

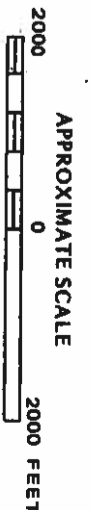
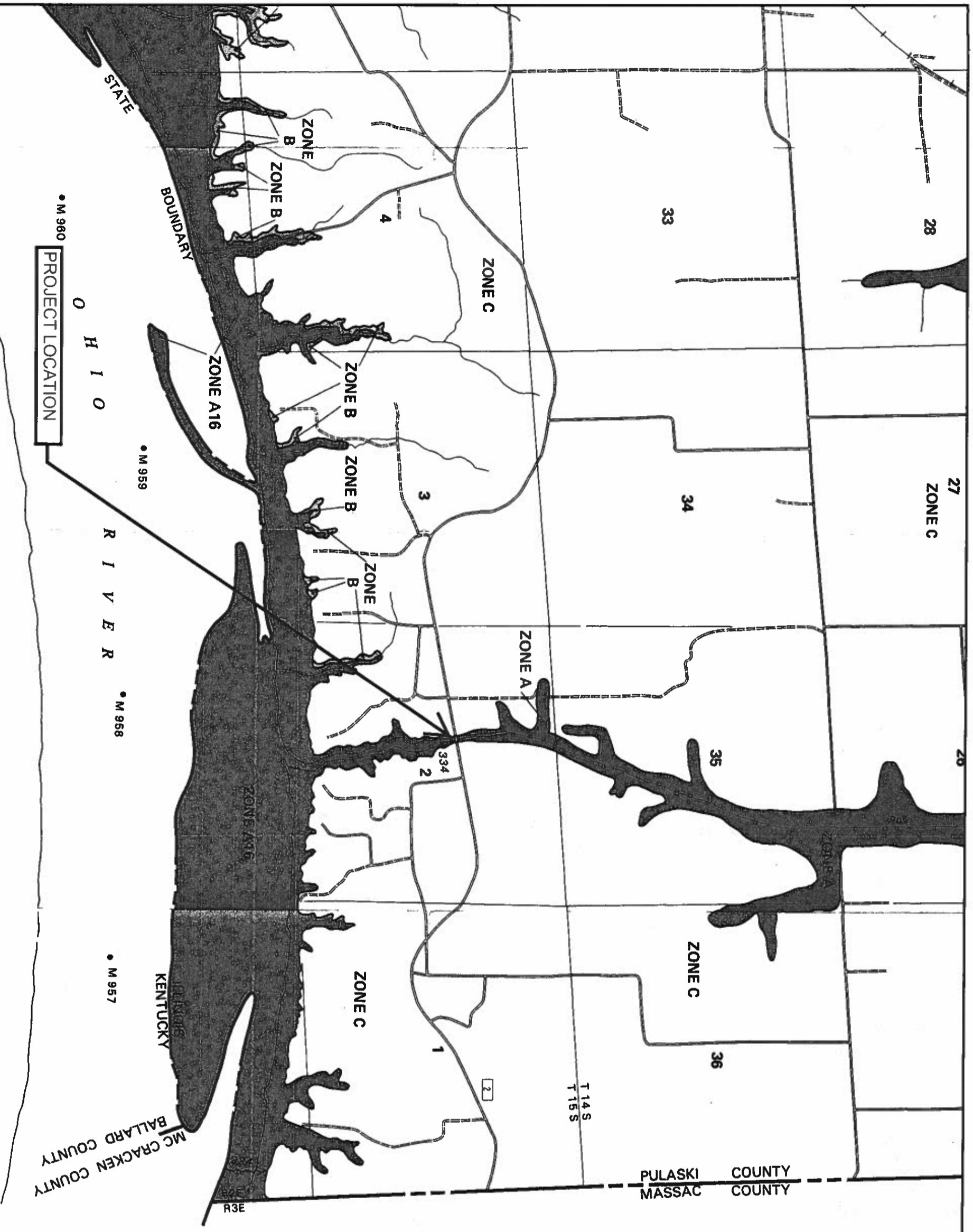
EXISTING STRUCTURE

TYPE: CIP Concrete Deck on Steel Girder
 LENGTH: 283'-0" Bk-Bk Abutments
 # SPANS: 3
 LOW BEAM: 360.07
 SKEW: 0
 LOW E.O.P.: 363.43

PROPOSED STRUCTURE

TYPE: 8" Concrete Deck on 54" PPC I Beams
 LENGTH: 274'-0" Bk-Bk Abutments
 # SPANS: 3
 LOW BEAM: 361.25
 SKEW: 0
 LOW E.O.P.: 366.02

NOTE:



NATIONAL FLOOD INSURANCE PROGRAM

FIRM
FLOOD INSURANCE RATE MAP

PULASKI COUNTY,
ILLINOIS
(UNINCORPORATED AREAS)

PANEL 50 OF 75

COMMUNITY-PANEL NUMBER
170562 0050 A

EFFECTIVE DATE:
MARCH 18, 1985



Federal Emergency Management Agency

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.fema.gov

Exhibit I



DEPARTMENT OF THE ARMY
U.S. ARMY ENGINEER DISTRICT, LOUISVILLE
CORPS OF ENGINEERS
REGULATORY DIVISION, SOUTH BRANCH
6855 STATE ROAD 66
NEWBURGH, INDIANA 47630
April 26, 2021

Regulatory Division
South Branch
ID No. LRL-2021-182-tmb

Mr. Brad Watson
Pulaski County Highway Department
P.O. BOX 97
Vila Ridge, IL 62996

Dear Mr. Watson:

This is in response to a Department of the Army permit application, received in this office February 22, 2021, requesting authorization to remove an existing three span steel beam/concrete deck bridge and replace it with a three span 54" beam concrete deck bridge with concrete column piers. All permanent impacts would occur above the ordinary high-water mark; however, temporary impacts would occur for stream crossings. The project is specifically located at 37.2403° N latitude and -88.9593° W longitude in Pulaski County, Illinois. The information supplied by you was reviewed to determine whether a Department of the Army (DA) permit will be required under the provisions of Section 10 of the Rivers and Harbors Act of 1899 or Section 404 of the Clean Water Act.

Your project is considered a discharge of backfill or bedding material for a road crossing. The project is authorized under the provisions of 33 CFR 330 Nationwide Permit (NWP) No. 14, Linear Transportation Projects, as published in the Federal Register January 6, 2017. Under the provisions of this authorization you must comply with the following:

1. Terms for Nationwide Permit No. 14
2. Nationwide Permit General Conditions
3. Water quality certification conditions issued by the Illinois Environmental Protection Agency published December 11, 2020

Special Conditions:

1. The Permittee shall implement all necessary precautions and measures so that any activity will not kill, injure, capture, harass, or otherwise harm any protected federally listed species. If the Permittee discovers or observes an injured/dead listed endangered or threatened species while accomplishing the authorized work, the Permittee shall immediately notify USACE to initiate the required Federal coordination. Tree clearing shall occur during the unoccupied time frame

Exhibit J

September 30 – April 1 of any given year for the Indiana bat, Gray bat and/or Northern long-eared bat.

2. Within 30 days from the date of completing the authorized work, the permittee shall restore any temporary stream impacts to pre-existing contours and elevations. The affected areas shall be re-vegetated, as appropriate to minimize erosion and ensure site stability.

This verification is valid until March 18, 2022. The enclosed Compliance Certification must be submitted to the District Engineer within 30 days of completion of the authorized activity or the implementation of any required compensatory mitigation, whichever occurs later. Note that we also perform periodic inspections to ensure compliance with our permit conditions and applicable Federal laws. A copy of this letter is being sent to your agent and to the ILEPA.

If you have any questions, please contact this office by writing to the above address, ATTN: CELRL-RD-S, calling 812-853-9713, or by email tre.m.barron@usace.army.mil. All correspondence pertaining to this matter should refer to our ID No. LRL-2021-182-tmb.

Sincerely



Tré M. Barron
Environmental Protection Specialist
Regulatory Division

Enclosures

Barron/RD-S/nwp14IL.doc

ADDRESS FOR COORDINATING AGENCY

Illinois EPA, Permits Section
P.O. BOX 19276
Springfield, IL 62794-9276

Mr. Larry Gowler
HMG Engineers, Inc.
9360 Holy Cross Lane
Breese, IL 62230

Compliance Certification:

Permit Number: LRL-2021-182-tmb

Name of Permittee: Mr. Brad Watson, Pulaski County Engineers

Date of Issuance: April 26, 2021

Upon completion of the activity authorized by this permit and any mitigation required by this permit, sign this certification and return it to the following address:

U.S. Army Corps of Engineers
CELRL-RD-S-TMB
6855 State Road 66
Newburgh, IN 47630

Please note that your permitted activity is subject to a compliance inspection by an U.S. Army Corps of Engineers representative. If you fail to comply with this permit you are subject to permit suspension, modification, or revocation.

I hereby certify that the work authorized by the above referenced permit has been completed in accordance with the terms and conditions of the said permit, and required mitigation was completed in accordance with the permit conditions.

Signature of Permittee

Date

Larry Gowler

From: Kranz, Jay C <Jay.Kranz@illinois.gov>
Sent: Tuesday, May 18, 2021 8:41 AM
To: Larry Gowler
Cc: Brad Watson
Subject: FW: PMA Seq. #22755 , 'Cleared for Design Approval' - Biological Form
Attachments: Species List_ Southern Illinois Sub-Office (1).pdf; NLAA Concurrence Verification Letter_ FHWA_ FRA_ FTA Programmatic Consultation for Transportation Projects affecting NLEB or Indiana Bat 20.pdf; Report of Possible Resource Conflicts.pdf; IDNR Response to Ecocat.pdf; NRR.pdf

FYI

A 'Cleared for Design Approval' date has been entered on the Biological form for the following PMA record: The project is cleared for letting with respect to Bio and wetlands.

Sequence #: 22755
District: 9
Route: FAS 937
Marked Route: CH 2
County: Pulaski

Please include the attached documentation with any permit applications. Please note the following commitments apply: These commitments are listed in the NRR.

To further ensure that the wetland and INAI sites will not be impacted, the following conservation measures shall be implemented:

1. The wetland and INAI site boundaries shall be added to the Contract Plan sheets and the contractor shall be notified that these areas are not to be entered or disturbed during construction. These areas are subject to Article 201.05 of IDOT's Standard Specifications for Road and Bridge Construction which identifies natural materials that are either to be saved, or are outside of the construction limits which are to remain undisturbed during construction.
2. These areas or boundaries may be clearly marked with flagging tape, or pay items may be included in the contract for temporary exclusionary fencing to be placed along the limits of construction as outlined in Section 201.05 (a) of IDOT's standard Specifications for Road and Bridge Construction.
3. Areas to be re-seeded shall utilize a Class 4 and 5 B wetland seed mix where feasible, and as outlined in IDOT's Standard Specifications for Road and Bridge Construction.

Additionally IDNR has recommended the following voluntary conservation measures for consideration during project design if feasible:

If temporary or permanent lighting is required, the Department recommends the following lighting recommendation to minimize adverse effects to wildlife, especially near natural areas:

- All lighting should be fully shielded fixtures that emit no light upward.

- Only “warm-white” or filtered LEDs (CCT < 3,000 K; S/P ratio < 1.2) should be used to minimize blue emission.
- Only light the exact space with the amount (lumens) needed to meet highway safety requirement.
- If LEDs are to be used, avoid the temptation to over-light based on the higher luminous efficiency of LEDs.

If erosion control blanket is to be used, the Department also recommends that wildlife-friendly plastic-free blanket by used to prevent the entanglement of native wildlife. If wildlife-friendly plastic-free blanket cannot be used, then plastic erosion control blanket should be removed once vegetation is established.

State of Illinois - CONFIDENTIALITY NOTICE: The information contained in this communication is confidential, may be attorney-client privileged or attorney work product, may constitute inside information or internal deliberative staff communication, and is intended only for the use of the addressee. Unauthorized use, disclosure or copying of this communication or any part thereof is strictly prohibited and may be unlawful. If you have received this communication in error, please notify the sender immediately by return e-mail and destroy this communication and all copies thereof, including all attachments. Receipt by an unintended recipient does not waive attorney-client privilege, attorney work product privilege, or any other exemption from disclosure.

From: Kranz, Jay C <Jay.Kranz@illinois.gov>
Sent: Tuesday, May 18, 2021 10:17 AM
To: Larry Gowler
Cc: Brad Watson
Subject: FW: 22755

Forgot to also include the following commitment::

Trees three (3) inches or greater in diameter at breast height shall not be cleared from April 1st through September 30th of any given year.

Kimberly Burkwald
Ecological Resource Specialist
IDOT Natural Resource Unit
(217) 836-4511
Kimberly.Burkwald@illinois.gov

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Illinois Department of Transportation

Memorandum

To: George A Tapas Attn: Greg S. Lupton
From: Jack A. Elston By: Thomas C. Brooks
Subject: Natural Resources Review
Date: May 5, 2021

A handwritten signature in cursive, likely of Thomas C. Brooks, written in black ink.

Tick Ridge Road
T 15S, R 2E, Sec 2
Pulaski County
Sequence # 22755

The proposed project is for the removal and replacement of the existing bridge, structure number 077-3000 carrying Tick Ridge Road over Post Creek Cutoff. During the planning stages, it was thought that up to 0.2 acres of right of way acquisition or temporary easement would be required to complete the project and the project was reviewed as such. With the current proposed ditch slopes and layout, the construction limits fit within the existing right of way, so no right of way acquisition will be necessary. There will be in stream work to Post Creek Cutoff. A total of 0.3 acres of trees will be removed.

Review for Illinois Endangered Species Protection and Illinois Natural Areas Preservation – Part 1075

The Illinois Natural Heritage Database contains records for several state and federally listed mussels immediately downstream of Post Creek Cutoff. However, these records occur in the Ohio River and per the Illinois Natural History Survey, there are no records, nor is there suitable habitat for mussels in Post Creek Cutoff for listed mussels as it is a concrete channel bottom. **Therefore, there will be no impact to any of these species.**

The Illinois Natural Heritage database does contain a record for the Post Creek Cutoff Illinois Natural Area Inventory (INAI) site in Southwest quadrant of the project. The site is a geological feature which is an exposure of Little Bear Soil. According to the Illinois Natural History Survey (INHS), the information in the Illinois Natural Heritage Database, and the sources cited in the INHS Wetland Delineation Report for this project, Little Bear Soil an ancient residual soil (paleosol) formed on Paleozoic rocks buried beneath Cretaceous and Tertiary sediments and is commonly found throughout the eastern Gulf Coastal Plain. However, in southern Illinois, the soil feature is at its northernmost extent (Mullen 1937; Pryor and Ross 1962). This soil feature is extremely rare in the state of Illinois.

Soil scientists from the Illinois Natural History Survey Wetland Science Program surveyed the site and found that this geological rarity was still present at the INAI site which was inventoried back in the 1970's. One of the greatest threats to the Little Bear Soil Feature would be blasting, excavation and earth moving which

could further expose the soil leaving it subject to erosion by natural elements. The INHS report states that the Little Bear Soil exists in immediate project vicinity, including at the existing bridge location underneath the existing bridge apron.

Plan sheets were provided depicting the INAI site boundaries and the location of the exposure within the INAI site and were determined to be outside the limits of construction and will not be impacted by the project. Additionally, the construction limits and project footprint have been minimized as much as possible to the extent that there will be no right of way acquisition necessary for the project. The area around Tick Ridge Road remains undeveloped and so the soil feature will continue to persist at this site so long as it remains unexposed to the natural elements which would cause it to erode.

During the wetland and soil survey, the State-endangered *Carex intumescens* was found in wetland site one. Wetland site one is adjacent to the INAI site in the Southwest quadrant of the project. This site was also depicted on the plan sheets and is outside the project's construction limit. Therefore, this office has determined that there will be no impact to this plant species.

To further ensure that the wetland and INAI sites will not be impacted, the following conservation measures shall be implemented:

1. The wetland and INAI site boundaries shall be added to the Contract Plan sheets and the contractor shall be notified that these areas are not to be entered or disturbed during construction. These areas are subject to Article 201.05 of IDOT's Standard Specifications for Road and Bridge Construction which identifies natural materials that are either to be saved, or are outside of the construction limits which are to remain undisturbed during construction.
2. These areas or boundaries may be clearly marked with flagging tape, or pay items may be included in the contract for temporary exclusionary fencing to be placed along the limits of construction as outlined in Section 201.05 (a) of IDOT's standard Specifications for Road and Bridge Construction.
3. Areas to be re-seeded shall utilize a Class 4 and 5 B wetland seed mix where feasible, and as outlined in IDOT's Standard Specifications for Road and Bridge Construction.

Therefore, consultation under Part 1075 is terminated.

This review for compliance with 17 Ill. Adm. Code Part 1075 is valid for two years unless new information becomes available that was not previously considered; the proposed improvement is modified; or additional species, essential habitat, or Natural Areas are identified in the vicinity. If the proposed improvement has not been implemented within two years of the date of this memorandum, or any of the above listed conditions develop, a new review will be necessary.

Review for Illinois Interagency Wetland Policy Act – Part 1090

The National Wetlands Inventory, Ducks Unlimited Wetlands Inventory, ground level and aerial photos, plan sheets, USDA soil maps, and topographic quadrangle maps were examined. Wetland surveys were conducted on June 9, 2020. One site was determined to be a wetland. As mentioned in the previous section the State-endangered *Carex intumescens* was found in this wetland during the survey. The project sponsor submitted a Wetland Impact Evaluation which shows that the wetland is outside of the project construction limits and will not be impacted by the project. **Therefore, the wetland review under Part 1090 is terminated.**

Review for Endangered Species Act - Section 7

The proposed improvement was reviewed in fulfillment of our obligation under Section 7(a)2 of the Endangered Species Act. Our review included use of the US Fish and Wildlife Service's Information for Planning and Conservation (IPaC) web-based review tool. Through IPaC, an official species list was received and is saved to the project folder. The list contains the endangered, threatened, proposed and candidate species and proposed and designated critical habitat that may be present within or in the vicinity of the proposed improvement. The following species are listed: Orange pimpleback, Rabbitfoot, Sheepnose (mussels), and Gray bat, Indiana bat (Ibat) and Northern long-eared bat (NLEB). There is no proposed or designated critical habitat in Pulaski County. **Under 50 CFR 402.12(e), the accuracy of the species list is limited to 90 days.**

Within IPaC there is a determination Key for the NLEB and Ibat. We used the key to determine applicability of the project with the USFWS revised programmatic biological opinion for transportation projects and to assess what effect the project would have on NLEB or Ibat. We completed an IPaC qualification interview and determined that **the project is within the scope of the programmatic biological opinion and is not likely to adversely affect either bat species provided the following conservation measure is implemented:**

Trees three (3) inches or greater in diameter at breast height shall not be cleared from April 1st through September 30th of any given year.

This determination is based in part on the results of the bat bridge assessment which showed no bats or signs of bats utilizing the bridge. **Please note that all bat bridge assessments are valid for two years and that expired assessments shall be updated prior to construction.**

We cross-referenced the preferred habitat of each of the remaining listed species with our knowledge of the project area and determined that the proposed improvement will have **no effect** on those species.

Should the project be modified or new information indicates listed or proposed species may be affected, consultation or additional coordination should be initiated.

KCB

Sequence #: 22755

Tick Ridge Road
077 3000 Post Crk Cutoff

Resource in Vicinity of Project Polygon

*INAI

*T&E

*Ducks Unlimited Wetlands

*National Wetlands Inventory

INAI & NP w/in 1 mile

*none found

No Resource Found

*Nature Preserve

*INHS Wetland







*Roadside Prairie Inventory

County: PULASKI

Section(PLSS): 3 15S2E2

Area: -0.0227 sq. miles = -14.52543 acres

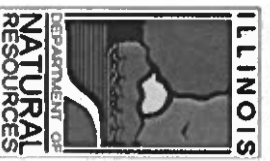
Report created by Burkwaldk

-  Threatened & Endangered Species (T&E)
-  Nature Preserve (NP)
-  Illinois Natural Areas Inventory (INAI)
-  Wetlands
-  INHS Wetland
-  Roadside Prairie Inventory



Include as additional documentation with permit applications* (USACE).





Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
www.dnr.illinois.gov

JB Pritzker, Governor
Colleen Callahan, Director

13 May 2021

Kimberly Burkwald
Illinois Department of Transportation BDE Central Office
Ecological Resource Specialist
2300 South Dirksen Parkway
Springfield, IL 62764

**RE: IDOT Local Agency Project 22755 Tick Ridge Road Bridge Replacement
Consultation Program
EcoCAT Review #2007113
Pulaski County**

Dear Mrs. Burkwald,

The Department has received your submission for this project for the purposes of consultation pursuant to the *Illinois Endangered Species Protection Act* [520 ILCS 10/11], the *Illinois Natural Areas Preservation Act* [525 ILCS 30/17], and Title 17 *Illinois Administrative Code* Part 860 and Part 1075. Additionally, the Department may offer advice and recommendations for species covered under the *Fish & Aquatic Life Code* [515 ILCS 5, et seq.]; the *Illinois Wildlife Code* [520 ILCS 5, et seq.]; and the *Herpiles-Herps Act* [510 ILCS 69].

The proposed project involves the for the removal and replacement of the existing bridge, structure number 077-3000, carrying Tick Ridge Road over Post Creek Cutoff in Pulaski County, Illinois.

The Illinois Natural Heritage Database contains records for state-listed butterfly (*Ellipsaria lineolata*), ebonyshell (*Reginaia ebenus*), elephant-ear (*Ellipito crassidens*), Ohio pigtoe (*Pleurobema cordatum*), purple warbyback (*Cyclonaias tuberculata*), and sheepsnose (*Plethobasus cyphus*) immediately downstream of Post Creek Cutoff in the Ohio River. However, per the Illinois Natural History Survey (INHS), there are no records, nor is there suitable habitat for mussels in Post Creek Cutoff for listed mussels as it is a concrete channel bottom. Therefore, the Department concurs with the NRR dated May 5, 2021 that adverse impacts are unlikely to these species.

During the wetland and soil survey, the state-listed *Carex intumescens* was found in wetland site one. Wetland site one is in the Southwest quadrant of the project. This site was also depicted on the plan sheets and is outside the project's construction limit. Therefore, the Department concurs with the NRR dated May 5, 2021 that adverse impacts are unlikely to this species.

The Illinois Natural Heritage database does contain a record for the Post Creek Cutoff Illinois Natural Area Inventory (INAI) site in Southwest quadrant of the project. The site is a geological feature which is an exposure of Little Bear Soil. Little Bear Soil an ancient residual soil (paleosol) formed on Paleozoic rocks buried beneath Cretaceous and Tertiary sediments and is commonly found throughout the eastern Gulf Coastal Plain. However, this soil feature is extremely rare in the state of Illinois. Plan sheets were provided depicting the INAI site boundaries and the location of the exposure within the INAI site and were determined to be outside the limits of construction and will not be impacted by the project. Additionally, the construction limits and project footprint have been minimized as much as possible to the extent that there will be no right of way acquisition necessary for the project. The area around Tick Ridge Road remains undeveloped and so the soil feature will continue to persist at this site so long as it remains unexposed to the natural elements which would cause it to erode.

To further ensure that the wetland and INAI sites will not be impacted, the IDOT is implementing the following conservation measures:

- The wetland and INAI site boundaries shall be added to the Contract Plan sheets and the contractor shall be notified that these areas are not to be entered or disturbed during construction. These areas are subject to Article 201.05 of IDOT's Standard Specifications for Road and Bridge Construction which identifies natural materials that are either to be saved, or are outside of the construction limits which are to remain undisturbed during construction.
- These areas or boundaries may be clearly marked with flagging tape, or pay items may be included in the contract for temporary exclusionary fencing to be placed along the limits of construction as outlined in Section 201.05 (a) of IDOT's standard Specifications for Road and Bridge Construction.

The Department concurs that these conservation measures will protect Post Creek Cutoff INAI site.

The Department has determined impacts to the other protected resources in the vicinity of the project location are unlikely.

The Department offers the following conservation measures to help protect native wildlife and enhance natural areas in the project area:

If temporary or permanent lighting is required, the Department recommends the following lighting recommendation to minimize adverse effects to wildlife, especially near natural areas:

- All lighting should be fully shielded fixtures that emit no light upward.
- Only "warm-white" or filtered LEDs (CCT < 3,000 K; S/P ratio < 1.2) should be used to minimize blue emission.
- Only light the exact space with the amount (lumens) needed to meet highway safety requirement.
- If LEDs are to be used, avoid the temptation to over-light based on the higher luminous efficiency of LEDs.

If erosion control blanket is to be used, the Department also recommends that wildlife-friendly plastic-free blanket be used to prevent the entanglement of native wildlife. If wildlife-friendly plastic-free blanket cannot be used, then plastic erosion control blanket should be removed once vegetation is established.

All disturbed areas should be reseeded with an appropriate native seed mix that contains forbs as well as grasses, where feasible.

The natural resource review reflects the information existing in the Illinois Natural Heritage Database at the time of the project submittal and should not be regarded as a final statement on the project being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are unexpectedly encountered during the project's implementation, the applicant must comply with the applicable statutes and regulations.

Please contact me with any questions about this review.

Sincerely,



Bradley Hayes
Resource Planner
Office of Realty & Capital Planning
Illinois Dept. of Natural Resources
One Natural Resources Way
Springfield, IL 62702-1271
Bradley.Hayes@Illinois.gov
Phone: (217) 782-0031



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Southern Illinois Sub-Office
Southern Illinois Sub-office
8588 Route 148

Marion, IL 62959-5822

Phone: (618) 997-3344 Fax: (618) 997-8961

<http://www.fws.gov/midwest/Endangered/section7/step1.html>



April 29, 2021

In Reply Refer To:

Consultation code: 03E18100-2021-1-0314

Event Code: 03E18100-2021-E-00950

Project Name: IDOT Local Agency Project 22755 Tick Ridge Road SN 077 3000 over Post Creek Cutoff

Subject: Concurrence verification letter for the 'IDOT Local Agency Project 22755 Tick Ridge Road SN 077 3000 over Post Creek Cutoff' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **IDOT Local Agency Project 22755 Tick Ridge Road SN 077 3000 over Post Creek Cutoff** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Gray Bat *Myotis grisescens* Endangered
- Monarch Butterfly *Danaus plexippus* Candidate
- Orangefoot Pimpleback (pearlymussel) *Plethobasus cooperianus* Endangered
- Rabbitsfoot *Quadrula cylindrica cylindrica* Threatened
- Sheepnose Mussel *Plethobasus cyphus* Endangered

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

IDOT Local Agency Project 22755 Tick Ridge Road SN 077 3000 over Post Creek Cutoff

Description

The proposed project is for the removal and replacement of the existing bridge, structure number 077-3000 carrying Tick Ridge Road over Post Creek Cutoff. There will be a total of 0.2 acres of right of way acquisition or temporary easement required to complete the project. There will be in stream work to Post Creek Cutoff. A total of 0.3 acres of trees will be removed.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See Indiana bat species profile

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See Northern long-eared bat species profile

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are all project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include any activities that are greater than 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include any activities within 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located within a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's summer survey guidance for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the national consultation FAQs.

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's summer survey guidance for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?
No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's summer survey guidance for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the summer survey guidance are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities within documented Indiana bat habitat^{[1][2]?}

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur within suitable but undocumented Indiana bat roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees within suitable but undocumented Indiana bat roosting/foraging habitat or travel corridors occur^{[1]?}

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities within documented NLEB habitat^{[1][2]?}

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur within suitable but undocumented NLEB roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees within suitable but undocumented NLEB roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will any tree trimming or removal occur within 100 feet of existing road/rail surfaces?

Yes

19. Will any tree trimming or removal occur between 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?
Yes
21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?
No
22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?
No
23. Does the project include slash pile burning?
No
24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?
Yes
25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)
[1] See the Service's current summer survey guidance for our current definitions of suitable habitat.
Yes
26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?
[1] See User Guide Appendix D for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.
Yes

SUBMITTED DOCUMENTS

- *LA Bridge-Bat Inspection_20190919.pdf* <https://ecos.fws.gov/ipac/project/EEGEVVFJ5ZEHHDLKVOHBNJNMJAO/projectDocuments/101663127>

27. Did the bridge assessment detect any signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of any species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing any work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing permanent lighting?

No

29. Does the project include the removal, replacement, and/or maintenance of any structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of temporary lighting during the active season?

No

31. Will the project install new or replace existing permanent lighting?

No

32. Does the project include percussives or other activities (not including tree removal/trimming or bridge/structure work) that will increase noise levels above existing traffic/background levels?

No

33. Are all project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

34. Will the project raise the road profile above the tree canopy?

No

35. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

36. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

39. General AMM 1

Will the project ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

40. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word "trees" as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS' current summer survey guidance for our latest definitions of suitable habitat.

Yes

41. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

42. Tree Removal AMM 4

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.3

4. Please describe the proposed bridge work:
removal and replacement
5. Please state the timing of all proposed bridge work:
unknown likely warm season
6. Please enter the date of the bridge assessment:
09/19/19

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on April 22, 2021. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects. The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Southern Illinois Sub-Office

Southern Illinois Sub-office

8588 Route 148

Marion, IL 62959-5822

Phone: (618) 997-3344 Fax: (618) 997-8961

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

April 29, 2021

Consultation Code: 03E18100-2021-SLI-0314

Event Code: 03E18100-2021-E-00955

Project Name: IDOT Local Agency Project 22755 Tick Ridge Road SN 077 3000 over Post Creek Cutoff

Subject: Updated list of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project "may affect" listed species or critical habitat. Under the ESA, it is the responsibility of the Federal action agency or its designated representative to determine if a proposed action "may affect" endangered, threatened, or proposed species, or designated critical habitat, and if so, to consult with the Service further. Similarly, it is the responsibility of the Federal action agency or project proponent, not the Service to make "no effect" determinations. If you determine that your proposed action will have "no effect" on threatened or endangered species or their respective critical habitat, you do not need to seek concurrence with the Service. Nevertheless, it is a violation of Federal law to harm or harass any federally-listed threatened or endangered fish or wildlife species without the appropriate permit.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website

<http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website <http://www.fws.gov/midwest/endorangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List
- USFWS National Wildlife Refuges and Fish Hatcheries

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Southern Illinois Sub-Office
Southern Illinois Sub-office
8588 Route 148
Marion, IL, 62959-5822
(618) 997-3344

Project Summary

Consultation Code: 03E18100-2021-SLI-0314

Event Code: 03E18100-2021-E-00955

Project Name: IDOT Local Agency Project 22755 Tick Ridge Road SN 077 3000 over Post Creek Cutoff

Project Type: TRANSPORTATION

Project Description: The proposed project is for the removal and replacement of the existing bridge, structure number 077-3000 carrying Tick Ridge Road over Post Creek Cutoff. There will be a total of 0.2 acres of right of way acquisition or temporary easement required to complete the project. There will be in stream work to Post Creek Cutoff. A total of 0.3 acres of trees will be removed.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@37.2402528,-88.95930499000528,14z>



Counties: Pulaski County, Illinois

Endangered Species Act Species

There is a total of 7 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

1. NOAA Fisheries, also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Gray Bat <i>Myotis grisescens</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6329	Endangered
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Clams

NAME	STATUS
Orangefoot Pimpleback (pearlymussel) <i>Plethobasus cooperianus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/1132	Endangered
Rabbitfoot <i>Quadrula cylindrica cylindrica</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5165	Threatened
Sheepnose Mussel <i>Plethobasus cyphus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6903	Endangered

Insects

NAME

STATUS

Monarch Butterfly *Danaus plexippus*

Candidate

No critical habitat has been designated for this species.
Species profile: <https://ecos.fws.gov/ecp/species/9743>

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

USFWS National Wildlife Refuge Lands And Fish Hatcheries

Any activity proposed on lands managed by the National Wildlife Refuge system must undergo a 'Compatibility Determination' conducted by the Refuge. Please contact the individual Refuges to discuss any questions or concerns.

THERE ARE NO REFUGE LANDS OR FISH HATCHERIES WITHIN YOUR PROJECT AREA.



Illinois Department of Transportation Memorandum

To: Bureau of Local Roads Attn: Doug DeLong
From: Jack Elston By: Brad Koldehoff
Subject: Cultural Resources - No Historic Properties Affected Clearance
Date: April 1, 2020

Pulaski County
FAS 937, CH 2 Tick Ridge Road
Southeast of New Grand Change
Sec. 12-00071-00-BR
Seq. 22755

For the above referenced undertaking, IDOT's qualified Cultural Resources staff hereby make a "No Historic Properties Affected" finding pursuant to Section 106 of the National Historic Preservation Act.

This finding concludes the Section 106 process in accordance with the stipulations of the Programmatic Agreement Regarding Section 106 Implementation for Federal-Aid Transportation Projects in the State of Illinois, executed March 6, 2018 by FHWA, Illinois SHPO, IDOT and the Advisory Council on Historic Preservation.

No further cultural resources coordination is required for this undertaking, unless design modifications or new information indicate that historic properties may be affected. If so, then, additional coordination with my office is required.

A handwritten signature in black ink, appearing to read "Brad Koldehoff".

Brad H. Koldehoff
Cultural Resources Unit Chief
Bureau of Design & Environment

Exhibit K2

**PULASKI COUNTY
HIGHWAY DEPARTMENT**

P.O. Box 97
300 Dakota Road
Villa Ridge, IL 62996

March 30, 2021

Patricia Adams & Bernita Hubbard
204 Easter Road
Grand Chain, IL 62941

File Copy

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Dear Ms. Adams & Hubbard:

The purpose of this letter is to notify you that the Pulaski County Highway Department is presently in the preliminary engineering phase for the replacement of the existing bridge carrying CH 2 (Tick Ridge Road) over the Post Creek Cut-Off stream, see enclosed Location Map. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. This project is targeted for construction in 2022.

Based on County Assessor's records, you were identified as the owner of parcel 06-02-100-779 which is located along Tick Ridge Road, approximately four miles east of Grand Chain, Illinois. It is the policy of the Illinois Department of Transportation (IDOT) and the Pulaski County Highway Department to provide all interested persons an opportunity to become acquainted with highway proposals of concern to them and to allow those affected to express their view at those stages of a proposed project when the flexibility to respond to those views still exists. Our current practice is to contact each affected property owner directly.

The proposed improvements consist of building a new bridge on the existing horizontal alignment and an improved vertical profile with new shoulders and guardrail. Full closure of the roadway is anticipated for the duration of the project with access by local traffic only. At this time, existing right-of-way appears to be sufficient to complete the construction of the proposed improvements without acquisition of new right-of-way, however, that is subject to change as not all approvals have come in yet.

At the end of this letter is an area where you can indicate your comments, if any. You may also request further discussions with us, either via phone call or a personal meeting. Please indicate the appropriate response and return to us in the enclosed self-addressed envelope. We would appreciate receiving any comments by May 13, 2021.

Upon completion of the preliminary engineering phase, a report will be prepared describing the proposed work and submitted to IDOT for approval. After approval is received, we will proceed with the plan preparation phase and the land acquisition phase, if required. In the land acquisition phase, a representative of the Pulaski County Highway Department will contact you regarding any necessary property acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquisition is deemed necessary. Should you have any questions or wish to arrange a meeting to discuss this project in more detail, please feel contact to contact me at the Pulaski County Highway Department (618) 342-6208 or via email at bwatson@pulaskicountyil.gov.

Sincerely,

PULASKI COUNTY HIGHWAY DEPARTMENT



Brad Watson, P.E.
County Engineer

Enc.

cc: HMG Engineers, Inc.
File

Patricia Adams & Bernita Hubbard
204 Easter Road
Grand Chain, IL 62941

Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement – Pulaski County

Check the appropriate response:

_____ I have no comments at this time.

_____ I have noted my comments on the back of this page.

_____ Please call me. (Include telephone number and preferred day and time.)

_____ I would like to have a personal meeting to discuss this project.
Please call to arrange a date, time, and location.
(Include contact person and telephone number.)

Signature of Owner _____

_____ Date

**PULASKI COUNTY
HIGHWAY DEPARTMENT**

P. O. Box 97
300 Dakota Road
Villa Ridge, IL 62996

March 30, 2021

Curt & Kay Jones
1090 Laurel Knoll Ct.
Brentwood, TN 37027

File Copy

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Dear Mr. & Mrs. Jones:

The purpose of this letter is to notify you that the Pulaski County Highway Department is presently in the preliminary engineering phase for the replacement of the existing bridge carrying CH 2 (Tick Ridge Road) over the Post Creek Cut-Off stream, see enclosed Location Map. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. This project is targeted for construction in 2022.

Based on County Assessor's records, you were identified as the owner of parcels 06-02-100-005 and 06-02-200-013 which are located along Tick Ridge Road, approximately four miles east of Grand Chain, Illinois. It is the policy of the Illinois Department of Transportation (IDOT) and the Pulaski County Highway Department to provide all interested persons an opportunity to become acquainted with highway proposals of concern to them and to allow those affected to express their view at those stages of a proposed project when the flexibility to respond to those views still exists. Our current practice is to contact each affected property owner directly.

The proposed improvements consist of building a new bridge on the existing horizontal alignment and an improved vertical profile with new shoulders and guardrail. Full closure of the roadway is anticipated for the duration of the project with access by local traffic only. At this time, existing right-of-way appears to be sufficient to complete the construction of the proposed improvements without acquisition of new right-of-way, however, that is subject to change as not all approvals have come in yet.


At the end of this letter is an area where you can indicate your comments, if any. You may also request further discussions with us, either via phone call or a personal meeting. Please indicate the appropriate response and return to us in the enclosed self-addressed envelope. We would appreciate receiving any comments by May 13, 2021.

Upon completion of the preliminary engineering phase, a report will be prepared describing the proposed work and submitted to IDOT for approval. After approval is received, we will proceed with the plan preparation phase and the land acquisition phase, if required. In the land acquisition phase, a representative of the Pulaski County Highway Department will contact you regarding any necessary property acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquisition is deemed necessary. Should you have any questions or wish to arrange a meeting to discuss this project in more detail, please feel contact to contact me at the Pulaski County Highway Department (618) 342-6208 or via email at bwatson@pulaskicountyil.gov.

Sincerely,

PULASKI COUNTY HIGHWAY DEPARTMENT



Brad Watson, P.E.
County Engineer

Enc.

cc: HMG Engineers, Inc.
File

Curt & Kay Jones
1090 Laurel Knoll Ct.
Brentwood, TN 37027

Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement – Pulaski County

Check the appropriate response:

_____ I have no comments at this time.

_____ I have noted my comments on the back of this page.

_____ Please call me. (Include telephone number and preferred day and time.)

_____ I would like to have a personal meeting to discuss this project.
Please call to arrange a date, time, and location.
(Include contact person and telephone number.)

Signature of Owner _____

_____ Date

**PULASKI COUNTY
HIGHWAY DEPARTMENT**

P. O. Box 97
300 Dakota Road
Villa Ridge, IL 62996

March 30, 2021

Post Creek Cut Off
PO Box 39
Grand Chain, IL 62941

File Copy

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Dear Post Creek Cut Off:

The purpose of this letter is to notify you that the Pulaski County Highway Department is presently in the preliminary engineering phase for the replacement of the existing bridge carrying CH 2 (Tick Ridge Road) over the Post Creek Cut-Off stream, see enclosed Location Map. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. This project is targeted for construction in 2022.

Based on County Assessor's records, you were identified as the owner of parcel 06-02-100-003 which is located along Tick Ridge Road, approximately four miles east of Grand Chain, Illinois. It is the policy of the Illinois Department of Transportation (IDOT) and the Pulaski County Highway Department to provide all interested persons an opportunity to become acquainted with highway proposals of concern to them and to allow those affected to express their view at those stages of a proposed project when the flexibility to respond to those views still exists. Our current practice is to contact each affected property owner directly.

The proposed improvements consist of building a new bridge on the existing horizontal alignment and an improved vertical profile with new shoulders and guardrail. Full closure of the roadway is anticipated for the duration of the project with access by local traffic only. At this time, existing right-of-way appears to be sufficient to complete the construction of the proposed improvements without acquisition of new right-of-way, however, that is subject to change as not all approvals have come in yet.

At the end of this letter is an area where you can indicate your comments, if any. You may also request further discussions with us, either via phone call or a personal meeting. Please indicate the appropriate response and return to us in the enclosed self-addressed envelope. We would appreciate receiving any comments by May 13, 2021.

Upon completion of the preliminary engineering phase, a report will be prepared describing the proposed work and submitted to IDOT for approval. After approval is received, we will proceed with the plan preparation phase and the land acquisition phase, if required. In the land acquisition phase, a representative of the Pulaski County Highway Department will contact you regarding any necessary property acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquisition is deemed necessary. Should you have any questions or wish to arrange a meeting to discuss this project in more detail, please feel contact to contact me at the Pulaski County Highway Department (618) 342-6208 or via email at bwatson@pulaskicountyil.gov.

Sincerely,

PULASKI COUNTY HIGHWAY DEPARTMENT



Brad Watson, P.E.
County Engineer

Enc.

cc: HMG Engineers, Inc.
File

Post Creek Cut Off
PO Box 39
Grand Chain, IL 62941

Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement – Pulaski County

Check the appropriate response:

_____ I have no comments at this time.

_____ I have noted my comments on the back of this page.

_____ Please call me. (Include telephone number and preferred day and time.)

_____ I would like to have a personal meeting to discuss this project.
Please call to arrange a date, time, and location.
(Include contact person and telephone number.)

Signature of Owner _____

_____ Date

**PULASKI COUNTY
HIGHWAY DEPARTMENT**

P. O. Box 97
300 Dakota Road
Villa Ridge, IL 62996

March 30, 2021

Jeffrey A. Wright
4000 Tick Ridge Road
Grand Chain, IL 62941

File Copy

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Dear Mr. Wright:

The purpose of this letter is to notify you that the Pulaski County Highway Department is presently in the preliminary engineering phase for the replacement of the existing bridge carrying CH 2 (Tick Ridge Road) over the Post Creek Cut-Off stream, see enclosed Location Map. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. This project is targeted for construction in 2022.

Based on County Assessor's records, you were identified as the owner of parcel 06-02-100-006 which is located along Tick Ridge Road, approximately four miles east of Grand Chain, Illinois. It is the policy of the Illinois Department of Transportation (IDOT) and the Pulaski County Highway Department to provide all interested persons an opportunity to become acquainted with highway proposals of concern to them and to allow those affected to express their view at those stages of a proposed project when the flexibility to respond to those views still exists. Our current practice is to contact each affected property owner directly.

The proposed improvements consist of building a new bridge on the existing horizontal alignment and an improved vertical profile with new shoulders and guardrail. Full closure of the roadway is anticipated for the duration of the project with access by local traffic only. At this time, existing right-of-way appears to be sufficient to complete the construction of the proposed improvements without acquisition of new right-of-way, however, that is subject to change as not all approvals have come in yet.

At the end of this letter is an area where you can indicate your comments, if any. You may also request further discussions with us, either via phone call or a personal meeting. Please indicate the appropriate response and return to us in the enclosed self-addressed envelope. We would appreciate receiving any comments by May 13, 2021.

Upon completion of the preliminary engineering phase, a report will be prepared describing the proposed work and submitted to IDOT for approval. After approval is received, we will proceed with the plan preparation phase and the land acquisition phase, if required. In the land acquisition phase, a representative of the Pulaski County Highway Department will contact you regarding any necessary property acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquisition is deemed necessary. Should you have any questions or wish to arrange a meeting to discuss this project in more detail, please feel contact to contact me at the Pulaski County Highway Department (618) 342-6208 or via email at bwatson@pulaskicountyil.gov.

Sincerely,

PULASKI COUNTY HIGHWAY DEPARTMENT



Brad Watson, P.E.
County Engineer

Enc:

cc: HMG Engineers, Inc.
File

Jeffrey A. Wright
4000 Tick Ridge Road
Grand Chain, IL 62941

Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement – Pulaski County

Check the appropriate response:

_____ I have no comments at this time.

_____ I have noted my comments on the back of this page.

_____ Please call me. (Include telephone number and preferred day and time.)

_____ I would like to have a personal meeting to discuss this project.
Please call to arrange a date, time, and location.
(Include contact person and telephone number.)

Signature of Owner _____

_____ Date

Larry Gowler

From: Brad Watson <bwatson@pulaskicountyil.gov>
Sent: Thursday, July 8, 2021 6:34 AM
To: Larry Gowler
Subject: FW: Post Creek Cut-Off Bridge Replacement

Larry

Informational letters regarding the Post Creek Cut-Off bridge replacement on Tick Ridge Rd were mailed to adjacent property owners on March 30, 2021. We received questions/ comments from one property owner. Attached is email correspondence between the adjacent property owner and myself.

The property owner was satisfied with the answers received. If you have any questions or require additional information just let me know.

Have a great day.

Brad Watson
Pulaski County Engineer
(618)342-6208 office
(618)645-0769 cell

From: Brad Watson
Sent: Wednesday, April 7, 2021 12:35 PM
To: lwright2005 <lwright2005@gmail.com>
Subject: RE: Post Creek Cut-Off Bridge Replacement

Trucks will still be able to use the roadway. Not much we can do about that. Sorry

From: lwright2005 <lwright2005@gmail.com>
Sent: Wednesday, April 7, 2021 12:28 PM
To: Brad Watson <bwatson@pulaskicountyil.gov>
Subject: RE: Post Creek Cut-Off Bridge Replacement

Attention: This email was sent from someone outside of Pulaski County. Always use caution when opening attachments or clicking links from unknown senders or when receiving unexpected emails.

Thank you for getting back to me so quickly! I completely understand all of this...was just hopeful it might not be as big of an inconvenience. However, having at least 6 months of no traffic past our house will be nice!

The bridge definitely needs replaced and I am so glad that it will be happening. Once it is done, will it continue to be a truck route?

Have a great day!
Lacey

Sent from my Verizon, Samsung Galaxy smartphone

-----Original message -----

From: Brad Watson <bwatson@pulaskicountyil.gov>

Date: 4/7/21 9:01 AM (GMT-06:00)

To: Lacey Wright <lwright2005@gmail.com>

Subject: RE: Post Creek Cut-Off Bridge Replacement

Lacey

Unfortunately, the bridge will need to be completely closed. Staged construction would add approximately 1 million to the cost of the project. An additional million could have added two issues with funding. The majority is federally funded. The program tends to lean toward approval of bridges with smaller budgets. An additional million could have jeopardized the entire project approval for federal money. Also, we are looking at an 80/20 split. We have to account for 20% of the funding. The bridge replacement is estimated at a little over 3.1 million. We are required to fund approximately \$620,000. That amount is a strain in itself. Adding an additional \$200,000 to the local share would prevent the project from happening at all.

The project will take approximately 6 months. I have stressed to the design firm that the project is started at a time that reduces the burden on school travel, and utilizes all the time kids are out for summer. There are alternative routes that are reasonably close. We understand a detour will be a hardship, and apologize for the inconvenience.

At this point, the tentative letting is for January 2022. That would set us up to start around April most likely. There are various aspects of the project that require state approval, and the bids for construction should be within a certain amount of the estimate. Basically, there are factors that could delay the start.

Thank you for contacting us. If you have more questions feel free to ask.

Have a great day.

Brad Watson

Pulaski County Engineer

(618)342-6208 office

(618)645-0769 cell

From: Lacey Wright <lwright2005@gmail.com>

Sent: Tuesday, April 6, 2021 8:19 AM

To: Brad Watson <bwatson@pulaskicountyil.gov>

Subject: Post Creek Cut-Off Bridge Replacement

Attention: This email was sent from someone outside of Pulaski County. Always use caution when opening attachments or clicking links from unknown senders or when receiving unexpected emails.

Good morning Brad. My husband, Jeff Wright, and I reside at 4000 Tick Ridge Road and received your letter over the weekend in regard to the bridge replacement at the Post Creek Cut-Off. I do have a few questions.

I know that the project is set to begin sometime in 2022. My first question is whether or not the bridge is going to be completely closed or if a single lane of traffic will still be allowed through. Obviously, Jeff drives that way for work and our kids travel that way on the bus to get to school. Having the bridge completely closed with absolutely no traffic access will drastically impact, not only our lives, but also many others. Hopefully, it will be limited to one lane! I know the letter sent indicates "full closure of the roadway...with access by local traffic only." Also during this time of construction if a single lane is left open, I assume that semi traffic will not be allowed.

My next two questions are likely subject to change: 1) how long is the construction scheduled to last? and 2) when in 2022 is construction expected to begin? I realize that you may not be able to answer these at this point.

Thank you for your time!

Lacey and Jeff Wright

July 19, 2021

Pulaski Co. Sheriff's Office
500 Illinois Ave.
Mounds City, IL 62963

Attn: Sheriff Randy Kern

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Sheriff Kern:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

The project will consist of the construction a replacement bridge and over 600' of approach roadway work. Road closure is expected for the duration of the project.

A marked detour route will be in place for the project using IL Routes 37 and 169 along with Boaz Road in Massac County. You may find an alternate (unmarked) detour route such as using Routes IL 37, IL 169, and US 45 along with Massac CH 5 (Joppa, Portland, and Grand Chain Roads) around the project to be preferable, or other alternates that you might know. A preliminary copy of the detour map is enclosed for your information.

It would be appreciated if you could share this information with dispatchers and area first responders who might be impacted by this closure next year.

A response is not required from your office. However, you are welcome to call HMG or the Pulaski County Highway Department (office: (618) 342-6208) if you have any questions, or if any additional information is required. I can be reached via phone at (618) 526-9611 Ext. 301 or via email at lgowler@hmgengineers.com.

Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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9360 Holy Cross Lane
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(F) 866.666.7213

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Murphysboro, IL 62966
(P) 618.684.9355
(F) 618.565.1079

Metropolis
605 Market Street
Metropolis, IL 62960
(P) 618.524.1085
(F) 618.524.3902

www.hmgengineers.com

July 19, 2021

Grand Chain Volunteer Fire Department
PO Box 3
600 South Main St.
Grand Chain, IL 62941

Attn: Fire Chief

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Chief:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

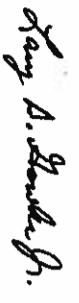
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A response is not required from your office. However, you are welcome to call HMG or the Pulaski County Highway Department (office: (618) 342-6209) if you have any questions, or if any additional information is required. I can be reached via phone at (618) 526-9611 Ext. 301 or via email at lgowler@hmgengineers.com.

Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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(F) 618.524.3902

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July 19, 2021

Pulaski County Ambulance Department
75 Camo Clad Drive
Mounds, IL 62964

Attn: Tracie Hatford, Ambulance Director

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Ms. Hatford:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

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A response is not required from your office. However, you are welcome to call HMG or the Pulaski County Highway Department (office: (618) 342-6208) if you have any questions, or if any additional information is required. I can be reached via phone at (618) 526-9611 Ext. 301 or via email at lgowler@hmgengineers.com.

Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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(F) 618.524.3902

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July 19, 2021

Illinois State Police – District 22
1154 Shawnee College Road
Ullin, IL 62992

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

To Whom It May Concern:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

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A response is not required from your office. However, you are welcome to call HMG or the Pulaski County Highway Department (office: (618) 342-6208) if you have any questions, or if any additional information is required. I can be reached via phone at (618) 526-9611 Ext. 301 or via email at lgowler@hmgengineers.com.

Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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HMG
Engineers • Surveyor

July 19, 2021

Grand Chain Post Office
200 South Main St.
Grand Chain, IL 62941

Attn: Postmaster

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Postmaster:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

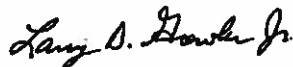
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Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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July 19, 2021

Century Unit School District #100
4721 Shawnee College Road
Ullin, IL 62992

Attn: Landon Sommer, Superintendent

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Mr. Sommer:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

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Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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July 19, 2021

Massac Co. Sheriff's Office
515 Market St.
Metropolis, IL 62960

Attn: Sheriff Chad Kaylor

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Sheriff Kaylor:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

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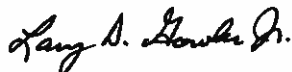
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It would be appreciated if you could share this information with dispatchers and area first responders who might be impacted by this closure next year.

A response is not required from your office. However, you are welcome to call HMG or the Pulaski County Highway Department (office: (618) 342-6208) if you have any questions, or if any additional information is required. I can be reached via phone at (618) 526-9611 Ext. 301 or via email at lgowler@hmgengineers.com.

Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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July 19, 2021

Joppa-Maple Grove CUSD #38
P.O. Box 10
911 North Avenue
Joppa, IL 62953

Attn: Dr. Vicki Artman, Superintendent

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Dr. Artman:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

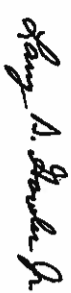
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Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

July 19, 2021

Massac County Unit School District #1
2841 Old Marion Road
Metropolis, IL 62960

Attn: Jason Hayes, Superintendent

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Mr. Hayes:

We are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

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Very truly yours,

HMG Engineers, Inc.

Larry D. Gowler, Jr.

Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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Metropolis
605 Market Street
Metropolis, IL 62960
(P) 618.524.1085
(F) 618.524.3902

www.hmgengineers.com

July 22, 2021

Robinson Transport Inc.
327 Fairgrounds Rd
Metropolis, IL 62960

Attn: Tina Williamson, Manager

Re: Tick Ridge Road (CH 2) over Post Creek Cut-Off Bridge Replacement
Pulaski County

Ms. Williamson:

In response to your request, we are writing to inform you of a bridge replacement project along Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over the Post Creek Cut-Off stream. The improvements begin approximately 415' west of the structure centerline and end approximately 485' east of the structure centerline. Construction is tentatively targeted to begin in the Spring of 2022 and last for approximately six (6) months, barring unforeseen conditions.

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Very truly yours,

HMG Engineers, Inc.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., Pulaski County Engineer (Letter Only)
HMG File 7073.1

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RHMG Engineers, Inc.
Mundelein, IL

Assturian Eason &
Associates, Inc.
Carbondale, IL

Coordination Meeting for Pulaski Co. Bridge Project

Date and Time: Tuesday October 23, 2012, 1:30 p.m.
Projected Attendance: Stacy Earnhart, Pulaski County Engineer
Chris Fraley, FHWA
Gary Galecki, IDOT
Richard Melcher, IDOT BLR District 9
Brad Hummert, HMG Engineers, Inc.
Brian Giltner, HMG Engineers, Inc.

Meeting Agenda Items:

1. Discussed options for the bridge rehabilitation/replacement
 - a. Remove existing deck and replace with a new cast-in-place concrete deck. This option would re-use existing. Because the existing paint on the steel beams is lead based, this would require the paint to be removed as part of the project.
 - b. Remove the existing superstructure and replace with new steel beam (the new outside beams would be located in the same locate as the existing outside beams), cast-in-place concrete deck (with 3'-1" overhang) with New Jersey barriers. This option would provide 26'-0" clear from face of barrier to face of barrier which meets the 3R criteria for this road. This option would require IDOT to agree to the abbreviated seismic evaluation. Must ensure that the new superstructure does not increase the dead load to the substructure by more than 10%.
 - c. Complete replacement of the bridge.
2. Concerns/issues for this site:
 - a. High seismic area
 - b. Crossing a major channel/ditch. Approximately 1 mile from the Ohio River.
3. Discussed funding options.
 - a. Pulaski County has limited funding available.
 - b. The size and cost of the project would probable make this project eligible for Major Bridge project funding.
4. Next Steps
 - a. Stacy Earnhart will coordinate with Lance Gribble (IDOT-BLR Dist. 9) on getting the bridge programmed for Federal funding.
 - b. It was suggested that after programming that the structure survey, hydraulic survey and the Bridge Condition Report be developed for this structure.

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9360 Holy Cross Lane
Breese, IL 62230
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(F) 866.666.7213

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METROPOLIS OFFICE
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Metropolis, IL 62960
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MURPHYSBORO OFFICE
1032 N. 6th Street
Murphysboro, IL 62966
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(F) 618.565.1079

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September 18, 2019

Mr. Keith Roberts, P.E.
Acting Region Five Engineer
Illinois Department of Transportation
Division of Highways / District 9
P.O. Box 100
Carbondale, Illinois 62903-0100

Attn: Mr. Jay Kranz
District 9 Local Roads & Streets Field Engineer

Re: Coordination Meeting Minutes
Major Bridge Program
FAS 937 (CH 2/Tick Ridge Road) over Post Creek Cut-Off Bridge Replacement
Section 12-00071-00-BR
Existing SN 077-3000
Pulaski County

Dear Mr. Kranz:

Non-Verbatim Meeting Minutes

September 10, 2019 – 8:00 am

IDOT District 9

Carbondale, IL

Minutes prepared by HMG Engineers, Inc.

The following attended the above-referenced Coordination Meeting (DeLong via video conference call):

Pulaski County: P.O. Box 97, 300 Dakota Road, Villa Ridge, IL 62996

Brad Watson – County Engineer

bwatson@pulaskicountyil.gov

Illinois Department of Transportation (IDOT):

J. Travis Emery – D9 Local Roads

James.Emery@illinois.gov

Jay Kranz – D9 Local Roads

Jay.Kranz@illinois.gov

Doug DeLong – Central Local Roads

Douglas.DeLong@illinois.gov

Federal Highway Administration (FHWA):

Chris Fraley

Chris.Fraley@dot.gov

HMG Engineers, Inc.: 9360 Holy Cross Lane, Breese, IL 62230

Brad Hummert – Chief Structural Engineer

bhummert@hmgengineers.com

Larry Gowler – Project Manager

lgowler@hmgengineers.com

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TRANSPORTATION
WASTEWATER
WATER SUPPLY

HMG
Engineers • Surveyors

Exhibit M2

Greetings and initial introductions were made, then Gowler provided a summary of the proposed project.

Project Outline:

- Gowler began by sharing an aerial image of the project showing a tangent horizontal alignment with the nearest curve around one-mile away.
 - The main concern is regarding the vertical alignment, which based on the existing plans appears to be a 30-35 mph design.
 - The existing bridge has a zero-grade per plans, between two sag curves followed by crest vertical curves; the zero-grade could be a contributing factor to the deterioration on the structure.
 - The proposed vertical alignment adds a crest curve over the bridge, between the adjacent sag curves in order to improve drainage off the bridge.
 - A roll plot of the vertical alignment was shown with the survey versus existing plan curves and showed some lower elevations due to settling, or other, on the nearest crest curves.
 - Trying to best-fit curves on the existing showed the flatter crest curves rate closer to 45-50 mph near where the proposed project ends.
 - Two proposed vertical alignments were also shown on the roll plot, one 45 mph and a higher one at 50 mph design speed.
- This meeting was requested to get early concurrence on design speed as it greatly impacts this bridge and project design.
- The Plan and Profile initially submitted was sag-crest-sag design at basically a 45-45-45 mph Design Speed.
- Hummert shared two concerns about higher design speeds:
 - Seismic: the bridge is near the highest seismic levels in the State, so raising the bridge more leads to higher design forces.
 - The project is also trying to salvage as much of the large existing slope wall as possible, so higher fills make that more difficult.

Discussion:

- **Design Policy:**
 - Gowler said this project has Major Bridge Funds and asked if there were any funding restrictions on using 3R Design Policy.
 - DeLong stated that 3R is preferred for this type of project.
 - Gowler said that since the crest curve over the bridge is new and the sags are improvements to existing ones, a design closer to 40-50-40 mph Design Speed could be investigated for impacts of the new crest curve meeting policy.
 - Kranz recommended that this project be processed as a 3R Project. DeLong and Fraley concurred.
- **Travel Speed:**
 - DeLong asked about the travel speed.
 - Discussed that the Speed Limit is 55 mph, and many travel it at 55 or higher, including trucks. Though Watson said that most do slow down at the bridge due to the bump at the bridge, etc.

- Discussed that advisory/warning signs are up to the County.
- Construction Traffic:
 - Gowler replied that the road would be Closed to Traffic and Detoured.
 - Discussed a marked detour route using IL Routes 169 and 37.
 - Watson mentioned Hillerman Road in Massac County as an option.
 - Discussed that it'd probably be best to keep trucks off other County roads, especially since would need to use Massac County's roads too; so, a Marked Detour using State Routes (IL Routes 169 & 37 and US 45) would be best and assume that local residents would find their best routes.
- ROW:
 - DeLong asked if ROW would be needed.
 - Gowler replied that it's uncertain at this point – existing ROW is wide around the bridge, but new ROW might be needed at the ends, depending upon the final vertical alignment.
- Categorical Exclusion:
 - Kranz recommended that the project be processed as State-Approved CE. DeLong and Fraley concurred.
 - PDR will be processed as a State-Approved CE with full report (BLR 22211), unless later findings require a change to BLR 22210.
 - Fraley later said that even if the bridge was found to be historical, he didn't think the processing would have to change.
- ESR:
 - An ESR for this project has not been submitted yet due to uncertainties in project limits and ROW needs.
 - All agreed that early submittal would be beneficial, and the desire to show enough area for the project limits to avoid resubmittals while not showing too much area that might trigger unnecessary environmental actions/paperwork.
 - DeLong asked about historical significance.
 - The bridge isn't currently listed historical, but it is an old structure, much older than the >40-yr old ESR submittal requirement.
 - Discussion that existing concrete bridge rail (Texas rail) could flag it – some had seen it happen before.
 - Don't want the ESR to delay project because of survey times.
 - Discussion that a tree clearing restriction is likely.
 - It was recommended that Watson perform a Bat Inspection as allowed by the County Engineer before submitting the ESR and including the Bat Inspection Report as an attachment to the ESR.
 - The Environmental reviewers might still end up doing their own survey, but it could save time due to allowable timeframes.

- Public Involvement:
 - Fraley said that the County needs to coordinate with emergency services (police, fire, ambulance, etc.) and school districts to inform them that there will be a full closure during construction so that they can plan accordingly. Due to the proximity to, and being a main route to/from Massac County, Massac County schools and services should also be notified. A road closure notice should be placed in a local paper, etc.
 - Watson said that he has a Facebook page which is popular – agreed that would be acceptable.
 - A Public Meeting will not be required for this project.
 - DeLong offered to email Watson some tips/advice on satisfying Public Involvement requirements for Federal jobs.
- Letting/Time Schedule:
 - Discussion about the project's construction time.
 - Gowler said a schedule hasn't been developed yet.
 - Hummert noted some of the uncertainties yet, such as planning on a drilled shaft foundation, but there have been problems and delays in getting the borings so unsure of number or size, etc.
 - Led into discussion of the bridge rail.
 - Existing rail is a Texas-style open concrete rail, and Major Bridge Application had included a similar, crash-tested Texas rail.
 - Emery brought up concerns for the County of maintaining the rail if hit and extra construction time.
 - Hummert said that it could be changed to a solid parapet rail for submittals, then the Texas rail could be offered, if needed, during historical review/approval.
 - DeLong asked about the letting date.
 - Gowler said it's a FY22 project, so aiming for an August 2021 letting in order to get clearances but could be adjusted as necessary.
 - Fraley advised scheduling the letting by considering the allowable construction period and work items.
 - Watson said he'd like to see as much constructed over the summer while school is out.
 - Hummert noted that project is at the mercy of the Ohio River, so the need for cofferdams will be investigated.
 - Gowler explained that the proposed piers are inside of the existing, which equates to about 4.5' difference in water level, so more susceptible than now.
 - Discussed different ideas for compressing the construction schedule; such as demo restrictions based on beam casting, etc.
 - Noted that lead times will likely increase with promised State spending.

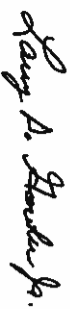
- Miscellaneous:
 - DeLong asked if Watson would be doing the inspection and if Construction Staking was to be a bid item.
 - Watson said he'd have a consultant for the inspection on this project.
 - Discussion about advantages of having the Contractor responsible for its own staking, etc.
 - Watson asked about sealing slope wall as he knew some joints were open and taking water, as water from the drains didn't make it all the way down.
 - Hummert thought something for it was in the estimate, but it could be covered in the contract.
 - Discussion about possible undermining. One idea was to sound the concrete.
 - Watson asked about the waterline on the bridge.
 - Gowler stated that it was shown on some drawings and hadn't been forgotten but the water company hadn't been contacted yet.
 - Watson said he'd been told that they could shut down that part during construction, but all agreed that it'll probably have to go back on the bridge after the new bridge is up as the most economical method.

If any discrepancies are found in the above minutes from the Coordination Meeting, please inform our office immediately so that any necessary corrections may be made.

Please feel free to contact us should you have any questions or if any additional information is required.

Very truly yours,

HMG Engineers, Inc.

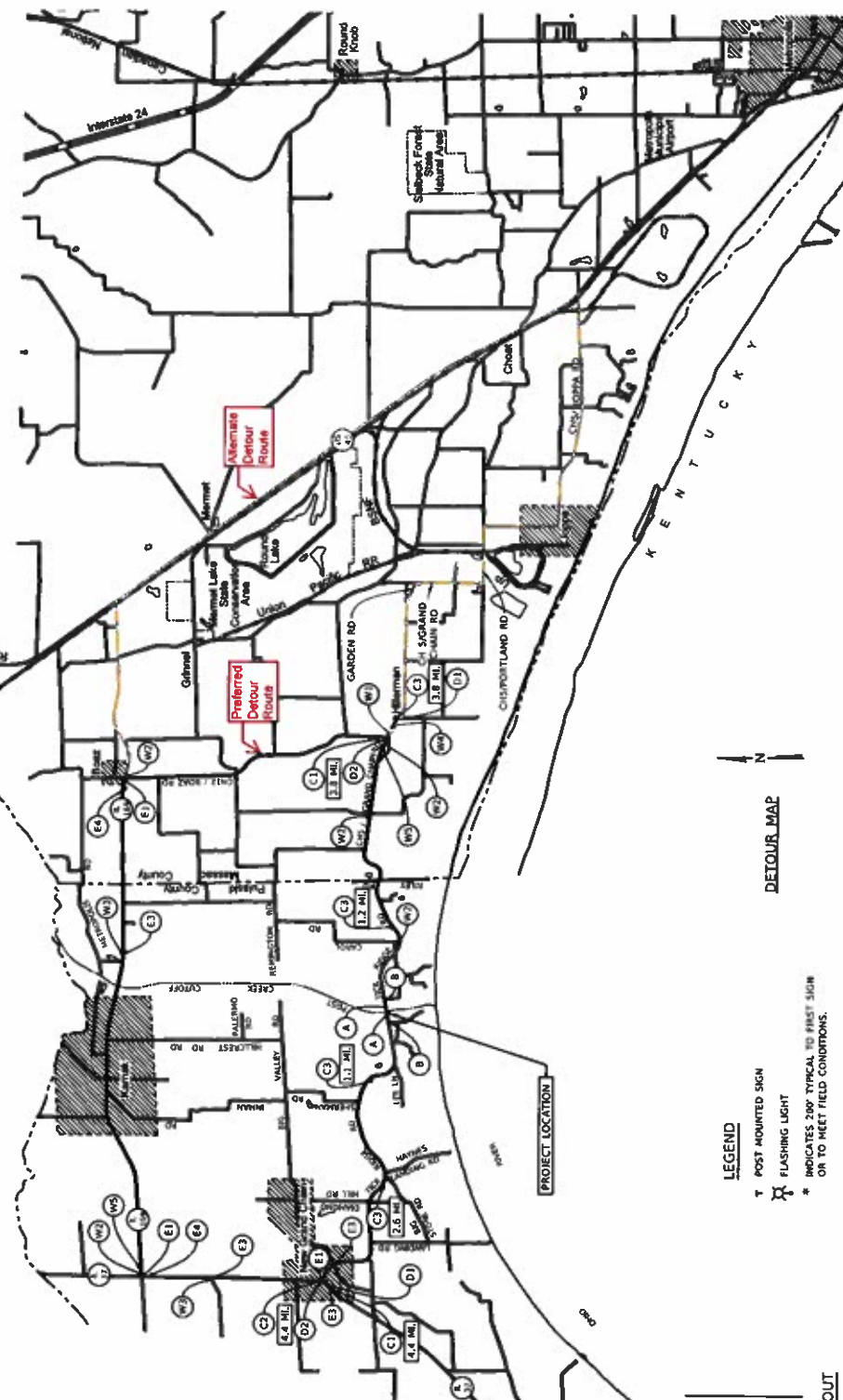
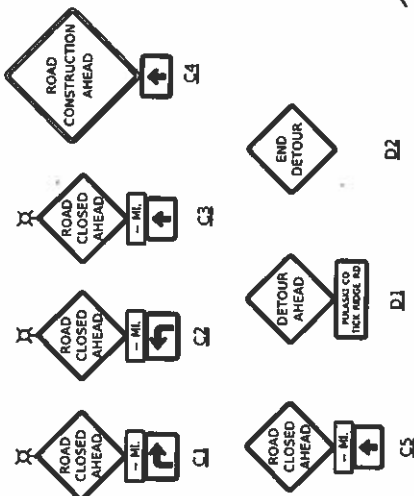
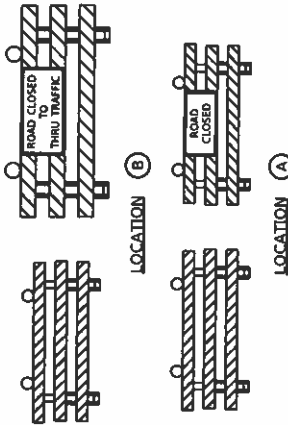


Larry D. Gowler, Jr., P.E., S.E.
Project Manager

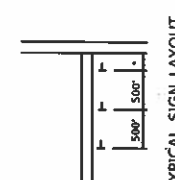
cc: Attendees via email
HMG File 7073.1

NOTES:

- ENGINEER MAY MODIFY SIGN PLACEMENT TO MEET FIELD CONDITIONS.
- ALL ADVANCED WARNING SIGNS SHALL BE 48" x 48" AND HAVE A BLACK LEGEND ON A FLUORESCENT ORANGE REFLECTORIZED BACKGROUND.
- ALL ADVANCED WARNING SIGNS SHALL INCLUDE LOW INTENSITY FLASHING LIGHTS.
- DETOUR SIGNING ASSEMBLY SHALL MAINTAIN THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN NO LESS THAN 5 FEET ABOVE THE EDGE OF PAVEMENT.
- ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE FOR STABILIZATION.
- AT LOCATIONS WHERE TYPE III BARRICADES ARE STAGGERED THE ROAD CLOSED TO THRU TRAFFIC SIGN SHALL BE PLACED ON THE FRONT BARRICADE.
- ALL ITEMS OF WORK INVOLVED WITH THE ROAD CLOSURE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LUMP SUM FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER 72 HOURS PRIOR TO CLOSURE.



LEGEND
 T POST MOUNTED SIGN
 FL FLASHING LIGHT
 * INDICATES 200' TYPICAL TO FIRST SIGN OR TO MEET FIELD CONDITIONS.



DESIGNED: _____ DRAWN: _____ CHECKED: _____ DATE: _____		PROJECT NO.: 11-00071-000-00 SHEET NO.: 11	
HMM CONSULTING, INC. 1000 N. WASHINGTON ST. CHICAGO, IL 60610		COUNTY: ILLINOIS SECTION: 937 SCALE: 1" = 1000'	
PROJECT NAME: I-24 CONTRACT NO.: 11-00071-000-00		TOTAL SHEETS: 11 SHEET NO.: 11	

Larry Gowler

From: Kranz, Jay C <Jay.Kranz@illinois.gov>
Sent: Tuesday, April 13, 2021 8:44 AM
To: Larry Gowler
Cc: Brad Watson
Subject: FW: Pulaski Co. - Sec. 12-00071-00-BR, FAS 937 (CH 2 - Tick Ridge Rd.) over Post Creek Cut Off
Attachments: Xerox Scan_04122021140811.pdf

Larry,
See message below and attachment for the detour.

Jay Kranz, P.E.
Local Roads & Streets Field Engineer
Illinois Department of Transportation
P.O. Box 100
Carbondale, IL 62903-0100
Phone: (618) 351-5268
Cell: (618) 967-7211

From: Stokes, Scott P <Scott.Stokes@illinois.gov>
Sent: Tuesday, April 13, 2021 7:05 AM
To: Kranz, Jay C <Jay.Kranz@illinois.gov>
Subject: RE: Pulaski Co. - Sec. 12-00071-00-BR, FAS 937 (CH 2 - Tick Ridge Rd.) over Post Creek Cut Off

They are using more signs than we would use for a state route detour. We do, however, use advance arrows along with right or left arrows when we approach a turn. They will just need to add these on the state route portions of the detour. I have attached an example.

From: Kranz, Jay C <Jay.Kranz@illinois.gov>
Sent: Wednesday, March 31, 2021 2:51 PM
To: Stokes, Scott P <Scott.Stokes@illinois.gov>
Subject: FW: Pulaski Co. - Sec. 12-00071-00-BR, FAS 937 (CH 2 - Tick Ridge Rd.) over Post Creek Cut Off

Scott,
Please review this detour. If approved they are wanting a letter/memo of the approval.
Thanks,

Jay Kranz, P.E.
Local Roads & Streets Field Engineer
Illinois Department of Transportation
P.O. Box 100
Carbondale, IL 62903-0100
Phone: (618) 351-5268
Cell: (618) 967-7211

From: Larry Gowler <lgowler@hmgengineers.com>
Sent: Wednesday, March 31, 2021 8:05 AM
To: Kranz, Jay C <Jay.Kranz@illinois.gov>
Cc: Brad Watson <bwatson@pulaskicountyil.gov>
Subject: [External] Pulaski Co. - Sec. 12-00071-00-BR, FAS 937 (CH 2 - Tick Ridge Rd.) over Post Creek Cut Off

Jay,

Please see attached request for usage of State routes as part of this project's detour and forward to the appropriate personnel.

We are looking for an approval letter/memo to include in the PDR.

Thanks,

Larry D. Gowler, Jr., P.E., S.E.
Project Manager

HMG Engineers, Inc.
9360 Holy Cross Lane
Breese, IL 62230
618.526.9611 Ext. 301
Fax 866.666.7213
Email: lgowler@hmgengineers.com

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March 31, 2021

SENT VIA EMAIL

Mr. Keith Roberts
Acting Regional Engineer
Illinois Department of Transportation
Division of Highways / District 9
P.O. Box 100
Carbondale, Illinois 62903-0100

Attn: Mr. Jay Kranz
Local Roads & Streets Field Engineer

Re: Illinois Special Bridge Program
FAS 937 (CH 2/Tick Ridge Road) over Post Creek Cut-Off
Section 12-00071-00-BR
Pulaski County
Detour Route Request

Dear Mr. Kranz:

Please forward this request to the appropriate District personnel.

We are requesting approval to use IL Rte. 37, IL Rte. 169, and US Rte. 45 as part of the detour route for the above-referenced project on behalf of Pulaski County. The project is on Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over Post Creek Cut-Off. We would like to detour traffic around the project using those routes in conjunction with Joppa, Portland, and Grand Chain Roads in Massac County.

The detour is anticipated to be in place over the Spring/Summer/Fall 2022 for around six (6) months, barring unforeseen conditions. Attached is a copy of the planned detour signage details for your consideration.

Your earliest response would be appreciated so it can be included in the Project Report.

Please advise if you need any additional information to process this submittal.

Very truly yours,

HMG ENGINEERS, INC.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

ENC.

cc: Brad Watson, P.E., County Engineer (via email)
HMG File 7073.1

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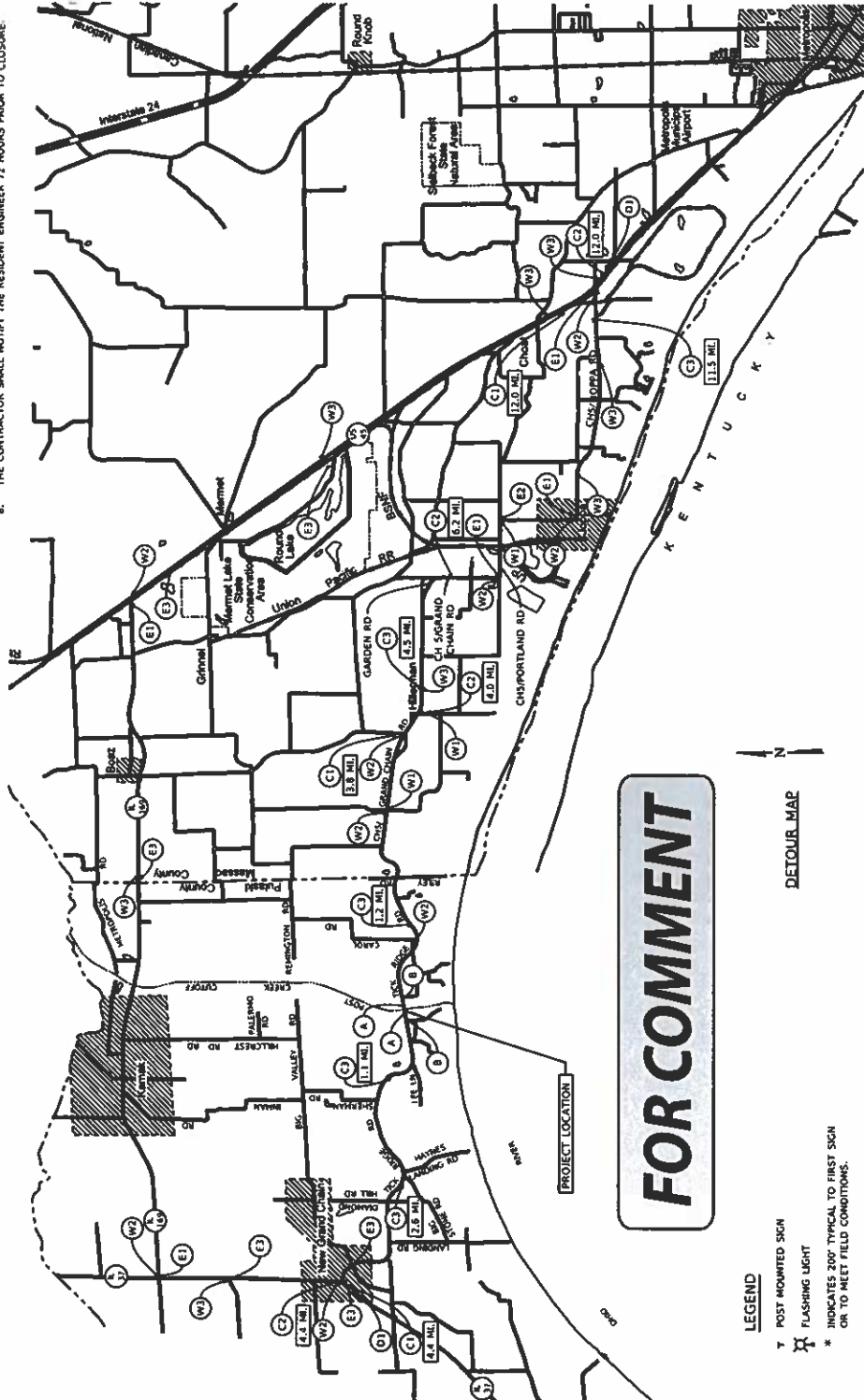
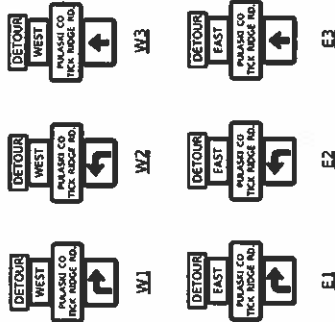
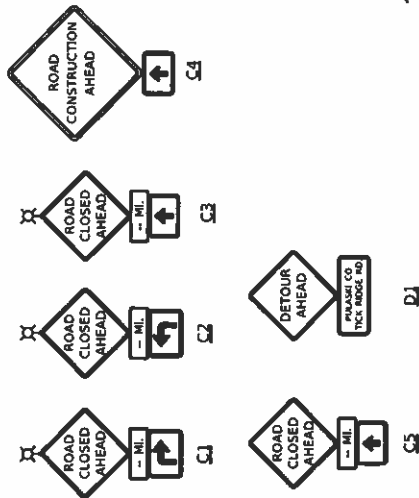
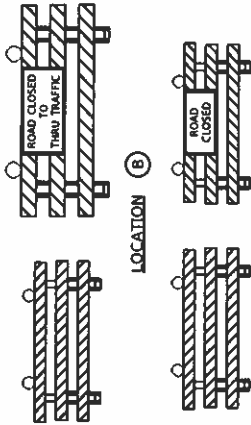
HMG Infrastructure
Services, LLC
Utility Operations

RHMG Engineers, Inc.
Mundelein, IL
Civil
Water/Wastewater
Surveying/GIS

Assturban Eaton &
Associates, Inc.
Carbondale, IL
Civil
M/E/P
Surveying

NOTES:

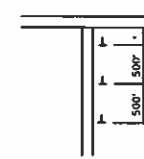
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- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER 72 HOURS PRIOR TO CLOSURE.



FOR COMMENT



LEGEND
 Y POST MOUNTED SIGN
 ✦ FLASHING LIGHT
 * INDICATES 200' TYPICAL TO FIRST SIGN OR TO MEET FIELD CONDITIONS.



TYPICAL SIGN LAYOUT

DESIGNED		REVISED		F.A.S.		SECTION		COUNTY		TOTAL SHEET	
DRAWN		REVISED		937		12-2007/100-VA		MADISON		11	
CHECKED		REVISED		SCALE:		SHEET 1 OF 1 SHEETS		STA. TO STA.		CONTRACT NO.	
DATE		REVISED		1/18/2008						100-000001	
FILE NAME: I:\2007\MADISON\CONTRACTS\100-000001\100-000001.dwg, PLOT DATE: 1/18/2008 USER NAME: jls PLOT SCALE: 1"=400.000' PLOT DATE: 1/18/2008 PLOT DATE: 1/18/2008											
HMG HERRING ENGINEERING, INC. 1000 W. MAIN ST. MADISON, KY 40138 (502) 261-1111											
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION						DETOUR MAP					

Larry Gowler

From: Massac HwyDept <massac.hwy.dept@outlook.com>
Sent: Thursday, April 1, 2021 10:14 AM
To: Larry Gowler
Subject: RE: Pulaski Co. - Tick Ridge Rd. (CH 2) over Post Creek Cut Off

Larry,

I talked with Brad yesterday about this plan. I really think you need to use Boaz Road which is our County Highway 12. This will cut out probably 25 signs and will shorten the detour by approx. 15-miles. If you have any questions, please call or e-mail. You can also check with Brad prior to revising.

Thanks,
Joe

MASSAC COUNTY HIGHWAY DEPARTMENT

2736 North Avenue
Metropolis, IL 62960
Phone: (618) 524-5227
Fax: (618) 524-5921
massac.hwy.dept@outlook.com
Joseph E. Matesevac, P.E.
County Engineer

From: Larry Gowler
Sent: Wednesday, March 31, 2021 8:07 AM
To: massac.hwy.dept@outlook.com
Cc: Brad Watson
Subject: Pulaski Co. - Tick Ridge Rd. (CH 2) over Post Creek Cut Off

Joe,

Please see attached request for usage of a Massac County route as part of this project's detour.

We are looking for an approval letter/memo/email to include in the PDR.

Feel free to call Brad Watson or myself if you have any questions.

Thanks,

Larry D. Gowler, Jr., P.E., S.E.
Project Manager

HMG Engineers, Inc.
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618.526.9611 Ext. 301
Fax 866.666.7213
Email: lgowler@hmgengineers.com

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March 31, 2021

Mr. Joseph Matesevac, P.E., County Engineer
Massac County Highway Department
2736 N. Avenue
Metropolis, IL 62960

SENT VIA EMAIL

Re: Illinois Special Bridge Program
FAS 937 (CH 2/Tick Ridge Road) over Post Creek Cut-Off
Section 12-00071-00-BR
Pulaski County
Detour Route Request

Dear Mr. Matesevac:

We are requesting approval to use Massac County Highway 5 (Joppa, Portland, and Grand Chain Roads) as part of the detour route for the above-referenced project on behalf of Pulaski County. The project is on Tick Ridge Road, approximately 4 miles east of Grand Chain, IL where CH 2 (Tick Ridge Road) crosses over Post Creek Cut-Off, and approximately 2 miles outside of Massac County. We would like to detour traffic around the project using that route in conjunction with IL Rte. 37, IL Rte. 169, and US Rte. 45.

The detour is anticipated to be in place over the Spring/Summer/Fall 2022 for around six (6) months, barring unforeseen conditions. Attached is a copy of the planned detour signage details for your consideration.

Your earliest response would be appreciated so it can be included in the Project Report.

Please advise if you need any additional information to process this submittal.

Very truly yours,

HMG ENGINEERS, INC.



Larry D. Gowler, Jr., P.E., S.E.
Project Manager

Enc.

cc: Brad Watson, P.E., County Engineer (via email)
HMG File 7073.1

Subsidiaries & Affiliates

HMG Constructors, LLC
Design-Build Services

HMG Infrastructure
Services, LLC
Utility Operations

RHMG Engineers, Inc.
Mundelein, IL

Civil
Water/Wastewater
Surveying/GIS

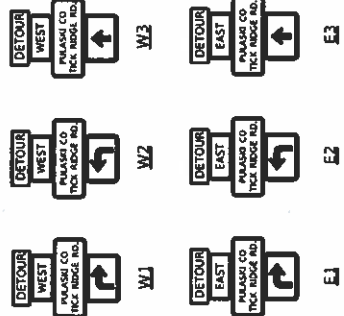
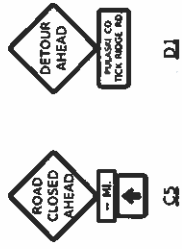
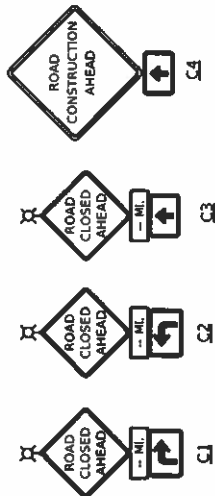
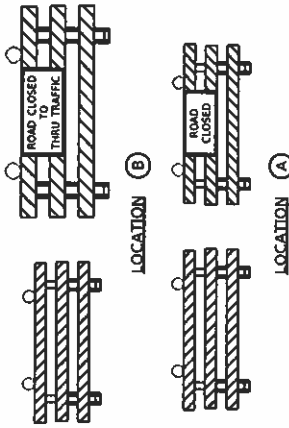
Asaturian Eaton &
Associates, Inc.
Carbondale, IL

Civil
M/E/P
Surveying

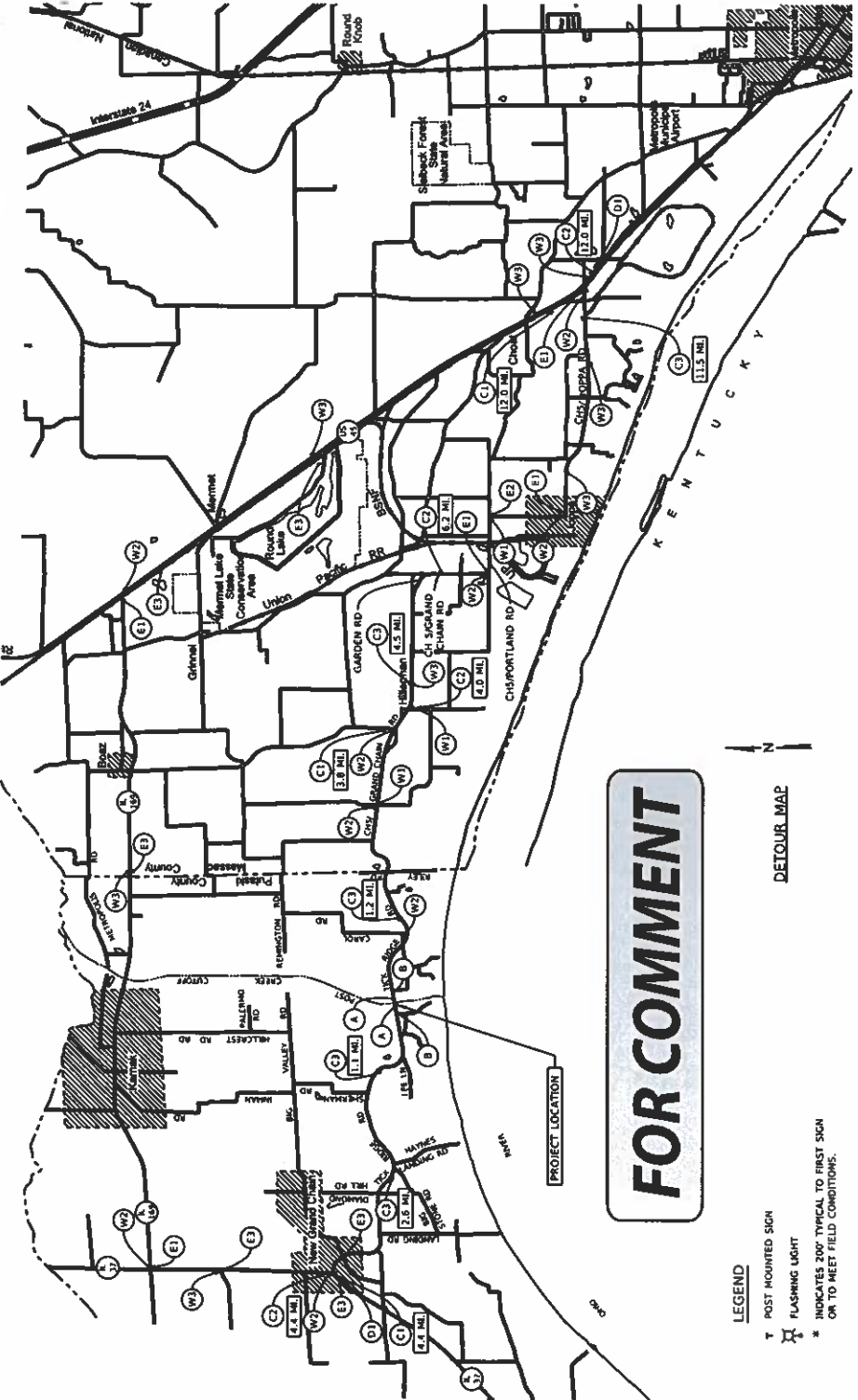
100% Employee-Owned

NOTES:

- ENGINEER MAY MODIFY SIGN PLACEMENT TO MEET FIELD CONDITIONS.
- ALL ADVANCED WARNING SIGNS SHALL BE 48" x 48" AND HAVE A BLACK LEGEND ON A FLUORESCENT ORANGE REFLECTORIZED BACKGROUND.
- ALL ADVANCED WARNING SIGNS SHALL INCLUDE LOW INTENSITY FLASHING LIGHTS.
- DETOUR SIGNING ASSEMBLY SHALL MAINTAIN THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN NO LESS THAN 5 FEET ABOVE THE EDGE OF PAVEMENT.
- ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE FOR STABILIZATION.
- AT LOCATIONS WHERE TYPE III BARRICADES ARE STAGGERED THE "ROAD CLOSED TO THRU TRAFFIC" SIGN SHALL BE PLACED ON THE FRONT BARRICADE.
- ALL ITEMS OF WORK INVOLVED WITH THE ROAD CLOSURE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LUMP SUM FOR TRAFFIC CONTROL AND PROTECTION. (SPECIAL).
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER 72 HOURS PRIOR TO CLOSURE.



FOR COMMENT

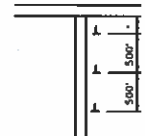


LEGEND

T POST MOUNTED SIGN

FLASHING LIGHT

* INDICATES 200' TYPICAL TO FIRST SIGN ON TO MEET FIELD CONDITIONS.



TYPICAL SIGN LAYOUT

DESIGNED	REVISION	F.A.S.	SECTION	COUNTY	TOTAL SHEETS
DRAWN	REVISED	937	17-00071-00-0A	PAKASAR	11
CHECKED	REVISED				
DATE	REVISED				
HMG HEADQUARTERS, INC. PROJECTS • CONSTRUCTION • MAINTENANCE • TRAFFIC CONTROL 1000 N. WASHINGTON ST. SUITE 1000 CHICAGO, ILLINOIS 60610 PHONE: (773) 399-1000 FAX: (773) 399-1001			STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DETOUR MAP SHEET 1 OF 1 SHEETS STA. TO STA. SCALE:		

Detailed Preliminary Cost Estimate

Estimated 274: Replacement 3-Span PCC I-Beam Bridge 28' Clear w/valved slopewall, conc. parapet & 626' Roadwork (900'± Project Length)			
Based on	0.3 ACRE	TREE REMOVAL ACRES	10,000.00
Prelim Estimates	1,600 CU YD	EARTH EXCAVATION	20.00
	1,050 CU YD	FURNISHED EXCAV	20.00
	0.75 ACRE	SEEDING	4,000.00
	0.75 ACRE	TEMP EROSION MEASURES	10,000.00
	1,481 SQ YD	PROCESSING MODIFIED SOIL 12"	30.00
	37 TON	LIME	200.00
	18 TON	SUB GRAN MAT A	100.00
	1,021 TON	AGG BASE CSE, TY A	30.00
	81 TON	AGGREGATE SHLDS A	30.00
	267 SQ YD	HMA SURF REM BUTT JT	25.00
	108 TON	HMA BC WIDENING	200.00
	392 TON	HMA BC IL-19.0 N50	150.00
	131 TON	HMA SC "C" N50	150.00
	88 TON	HMA SHOULDER	200.00
	100 SQ YD	BR APPR PVT CON (PCC)	250.00
	100 SQ YD	PAVEMENT FABRIC	20.00
	736 SQ YD	PAVEMENT REMOVAL	25.00
	333 SQ YD	PCC SHOULDER REMOVAL	25.00
	500 SQ YD	APPROACH SLAB REMOVAL	25.00
	2 EACH	PRC FLAR END SEC 30	1,500.00
	50 FOOT	PIPE CULVERTS, CLASS A, 30"	200.00
	460 FOOT	PAVED DITCH REMOVAL	10.00
	500 FOOT	PAVED DITCH TA-30	150.00
	280 FOOT	GUARDRAIL REMOV	15.00
	100 FOOT	SPBGR TYPE A	40.00
	4 EACH	TRAF BAR TERM T6	4,000.00
	4 EACH	TR BAR TRM T1 SPL TAN	3,500.00
	3,800 FOOT	PAINT PVT MK LINE 4	50.00
	1 L SUM	CONST STAKING	3.00
	1 EACH	TRAFFIC CONTROL PROT	25,000.00
	1 L SUM	Other misc items	25,000.00
		Road SubTotal	10,000.00
			\$541,690.00
Estimated	1 EACH	REM EXIST STRUCT	125,000.00
	225 SQ YD	SLOPE WALL REMOVAL	25.00
	250 CU YD	STRUCTURE EXCAVATION	50.00
	8 EACH	FLOOR DRAINS	550.00
	290 CU YD	CONC STRUCT	1,100.00
	350 CU YD	CONC SUP-STR	1,300.00
	958 SQ YD	BR DECK GROOVING	10.00
	1,410 SQ YD	PROTECTIVE COAT	2.00
	1,076 FOOT	F & E P CON I-BM 54	350.00
	157,000 POUND	REINF BARS, EPOXY CTD	1.50
	388 EACH	MECHANICAL SPLICERS	60.00
	225 SQ YD	SLOPE WALL 4 INCH	150.00
	360 FOOT	FUR STL PILE HP14X117	150.00
	360 FOOT	DRIVE STL PILE	1.00
	2 EACH	TEST PILE ST HP14X117	12,000.00
	8 EACH	PILE SHOES	150.00
	70 CU YD	DRILLED SHAFT IN SOIL	1,000.00
	30 CU YD	DRILLED SHAFT IN ROCK	4,000.00
	1 EACH	NAME PLATES	500.00
	58 FOOT	PREF JT STRIP SEAL	200.00
	2 EACH	COFFERDAM TYP 1	15,000.00
	60 SQ YD	GEOCOMPOSITE WALL DRAIN	25.00
	169 FOOT	PIPE UNDERDRAINS FOR STRUCTURES 4"	30.00
	124 CU YD	GRANULAR BACKFILL FOR STRUCTURES	60.00
	1,643 SQ FT	PRECAST BR APPR SLAB	40.00
	201 SQ YD	CONC WEARING SURFACE (VARIABLE)	120.00
	4 EACH	DRAINAGE SCUPPERS, DS-11	2,000.00
	1 L SUM	Other misc items	10,000.00
		Bridge SubTotal	\$1,910,375.00
Estimated	Calcd Bridge Rate:	\$187.61 \$/SF	

Estimate Subtotal	\$2,577,065.00
Contingency (0%)	\$0.00
Inflation (1 year @ 2.5% Ave)	\$64,426.62
Mobilization (6%)	\$158,489.50
Estimated Construction Cost FY2022	\$2,799,981.12

Larry Gowler

From: Kranz, Jay C <Jay.Kranz@illinois.gov>
Sent: Wednesday, July 7, 2021 1:11 PM
To: Larry Gowler
Cc: Brad Watson
Subject: FW: draft Project Development Report (PDR) Comments; Bridge Replacement Project CH 2 (Tick Ridge Road) over Post Creek Cut-off ; Section 12-00071-00-BR; Pulaski County

Larry,

See email below. Draft PDR is approved. Include this email with the final PDR and get it signed, send me 2 copies.
Thanks,

Jay Kranz, P.E.
Local Roads & Streets Field Engineer
Illinois Department of Transportation
P.O. Box 100
Carbondale, IL 62903-0100
Phone: (618) 351-5268
Cell: (618) 967-7211

From: DeLong, Douglas <Douglas.DeLong@illinois.gov>
Sent: Wednesday, July 7, 2021 12:22 PM
To: Salifu, Hateeyat N. <Hateeyat.Salifu@illinois.gov>; DOT.LocalAgencyESR <DOT.LocalAgencyESR@illinois.gov>
Cc: Kranz, Jay C <Jay.Kranz@illinois.gov>
Subject: draft Project Development Report (PDR) Comments; Bridge Replacement Project CH 2 (Tick Ridge Road) over Post Creek Cut-off ; Section 12-00071-00-BR; Pulaski County

Subject: draft Project Development Report (PDR) Comments; Bridge Replacement Project CH 2 (Tick Ridge Road) over Post Creek Cut-off ; Section 12-00071-00-BR; Pulaski County

PMA Sequence # 22755
Bridge Replacement Improvement Project in Pulaski County
Section 12-00071-00-BR
Lead Agency: Pulaski County

Jay,
I have comments listed below to be forwarded to the Local Agency. This email can serve as a list of comments from the Central Bureau of Local Roads & Streets (CBLRS) located in Springfield. I recommend that the District Land acquisition Engineer be contacted if it is learned that ROW is required to assure that the requirements of the Uniform Act have been met (see link to Uniform Act Presentation below). Report shows no ROW required.

After it is confirmed by the district Local Road Field Engineer, that none of the 14 potential unusual circumstance exceed the thresholds for the IDOT/FHWA Programmatic Agreement dated 10/14/2020; the environmental documentation of this project will be as a State Approved Categorical Exclusion to be

processed for approval as a district approved PDR on Form BLR 22211; then return to Jay Kranz (District 9 Local Roads Bureau) for signatures.

In General, The report is well prepared and after it was decided that no ROW would be required the environmental clearances were able to be expedited.

CBLRS PDR Comments

General/Administrative Comments

- Please submit most current set of plan & profiles with final PDR submittal.
- Provide assurance that the public and nearby landowner are aware of the planned construction schedule.
- After review of the FHWA Public Involvement video as provided in the link below confirm that the requirements of Public Involvement for NEPA have been met for this project.
- Determine the proposed letting date.
- Confirm that all district comments have been addressed.

Right-of-Way – Form LA 4194A and/or LA 4193

- When right-of-Way is acquired for federal-aid projects, full compliance with Titles II and III of the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970 is mandatory. Contract the district Land Acquisition Engineer for coordination of land acquisition and approval of personnel for land acquisition services if it is learned that temporary easements are necessary or permanent ROW is needed.

I am sending you this information to help you understand the requirements of the Uniform Relocation Assistance and Real Property Acquisitions Policies Act otherwise know as the Uniform Act. The link to the FHWA provided video link (see Below) will be useful to learn the requirements of the Uniform Act and when they apply. The title of this You Tube is "Introduction to Right-of-Way Requirements and the Uniform Act."

The Link to the uniform Act video presentation is 6:34 minutes:
<https://www.fhwa.dot.gov/federal-aidessentials/catmod.cfm?category=rightofw>

Public Involvement for this Project, BLRS Manual Chapter 21 – (PDR # 17)

- Confirm that sufficient public involvement has been held to satisfy NEPA requirements.
- Documentation of concerns identified by public with responses to comments that have been adequately addressed is required prior to FHWA or IDOT Categorical Exclusion approval.
- Here is a link to the requirements for NEPA Public Involvement. <https://www.youtube.com/watch?v=XxUjentDnew>

If you need any additional information or help processing this Project Development Report. Please email me at Douglas.Delong@illinois.gov.

Doug DeLong, P.E.
Project Development Engineer
Illinois Department of Transportation
Bureau of Local Roads & Streets
2300 South Dirksen Parkway
Springfield, IL 627

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